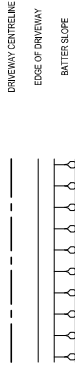
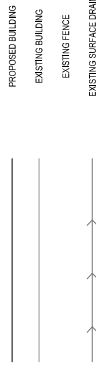


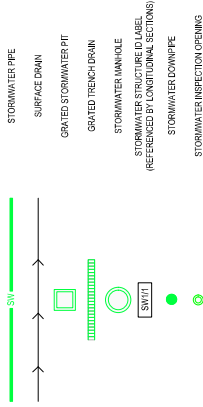
ROAD/DRIVEWAY



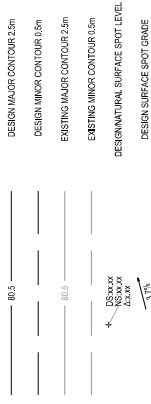
SITE



DRAINAGE



SURFACE



SLAB JOINTING



ABBREVIATIONS

- CHANGE MARKER ABBREVIATIONS: CH, CHANGE; TP, TANGENT POINT; TS, TANGENT SPIRAL; CS, CURVE SPIRAL; SC, SPIRAL CURVE. HYDRAULIC ABBREVIATIONS: OSD, ONSITE DETENTION; PVC, POLYVINYLCHLORIDE PIPE; RCP, REINFORCED CONCRETE PIPE; SW, STORMWATER; HDPE, HIGH DENSITY POLYETHYLENE PIPE. GENERAL ABBREVIATIONS: AHD, AUSTRALIAN HEIGHT DATUM; FFL, FINISHED FLOOR LEVEL; RL, REDUCED LEVEL (RELATIVE TO AHD); DS, DESIGN SURFACE; SL, SURFACE LEVEL; SL DIFF, SURFACE LEVEL DIFFERENCE (OUTFILL).

WATER SERVICES



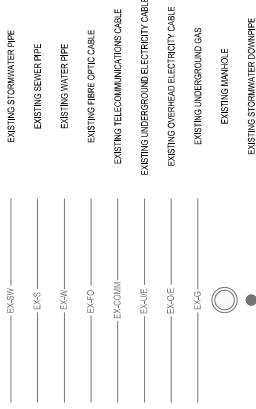
FIRE SERVICES



SEWER SERVICES



EXISTING SERVICES



Project Details: PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH. Drawing Name: LEGEND. Scale: -. Project No: 2023-02-05. Date Iss: C0001. Date Rev: D1. ACACIA ENGINEERING STRUCTURAL | CIVIL | HYDRAULIC. PO Box 670 Wagon Tugralia 7050. 0447 895 514. admin@acaciang.com.au www.acaciang.com.au License No. CC5891. No. Revision: 21032024. Date.



KEY PLAN
SCALE: 1:500

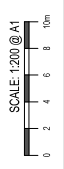
SCALE: 1:500 @ A1
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<p>Project Name KEY PLAN</p>	
<p>Scale 1:500</p>	<p>Sheet A1</p>
<p>Project No. 2023-02-05</p>	<p>Draw No. C1000</p>
<p>Date D3</p>	<p>Rev. D3</p>
<p>Project Description PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH</p>	
<p>Client GIC ESTATES PTY LTD</p>	
<p>PO Box 670 Woomeroo Tasmania 7250 0447 595 514 admin@acaciaeng.com.au www.acaciaeng.com.au Licence No. CC5891</p>	
<p>ACACIA ENGINEERING STRUCTURAL CIVIL HYDRAULIC</p>	
<p>No.</p>	<p>Date</p>
<p>D3</p>	<p>20/05/2024</p>
<p>D1</p>	<p>21/03/2024</p>
<p>No.</p>	<p>Revision</p>



EXISTING SITE PLAN - SHEET 1
SCALE 1:200

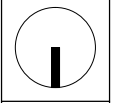
INSET A - ACCESS FROM JETTY ROAD
SCALE 1:200



Drawing Name	EXISTING SITE PLAN - SHEET 1		
Scale	1:200	Sheet No.	A1
Project No.	2023-02-05	Draw No.	C1001
Rev.		Rev.	D1

Project Details
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH

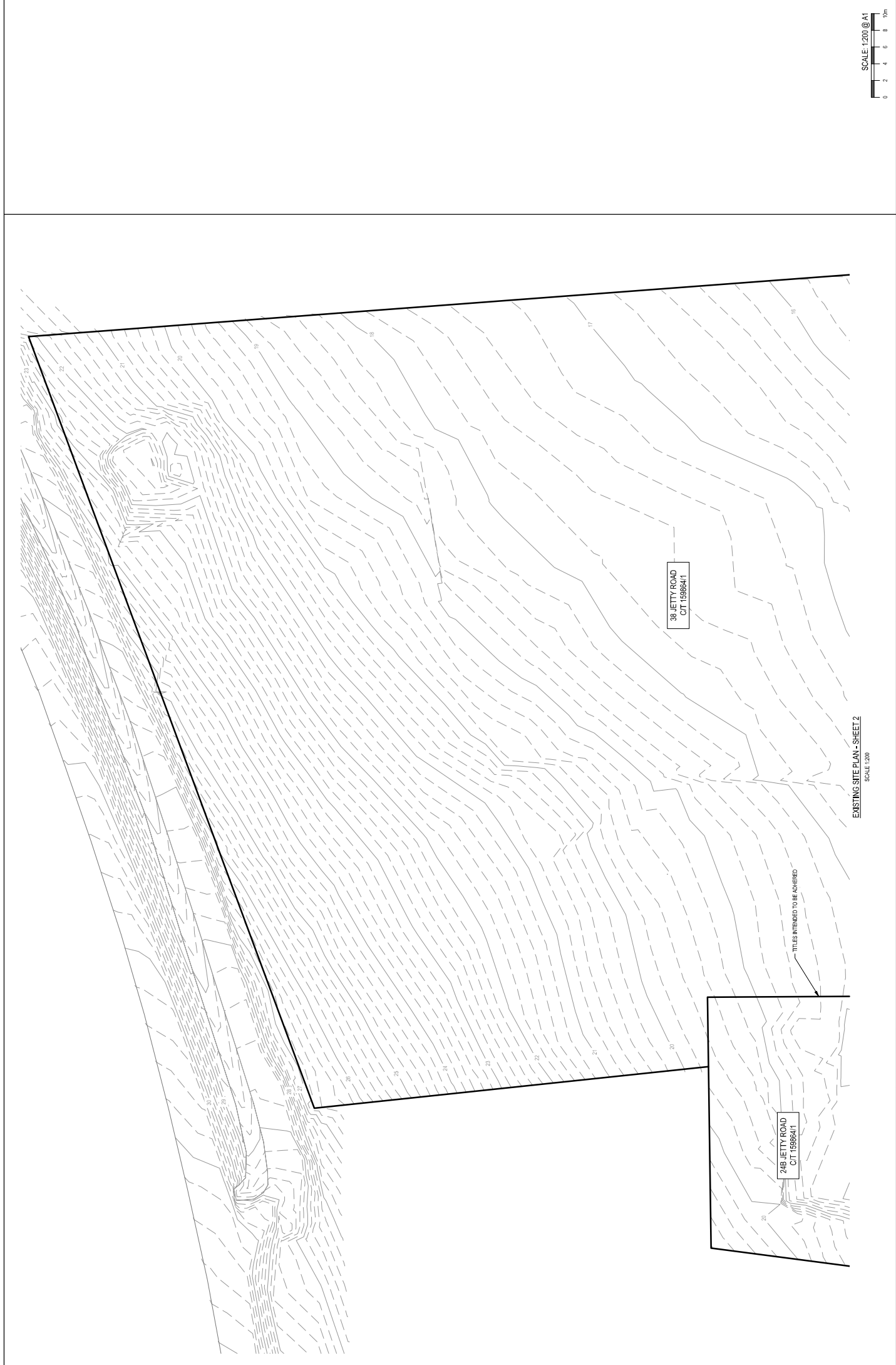
Client
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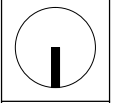
No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



Drawing Name		EXISTING SITE PLAN - SHEET 2	
Scale	1:200	Project No.	2023-02-05
Sheet	A1	Draw No.	C1002
		Rev.	D1

Project Name
**PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH**

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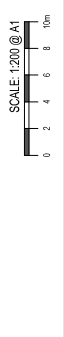
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No.	21032624
Revision	Date
D1	DEVELOPMENT APPROVAL



PROPOSED SITE PLAN - SHEET 1
SCALE 1:200

INSET A - ACCESS FROM JETTY ROAD
SCALE 1:200



Project Name	PROPOSED SITE PLAN - SHEET 1		
Scale	A1	Project No.	2023-02-05
Sheet	1:200	Draw No.	C1003_D3

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Client
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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024



PROPOSED SITE PLAN - SHEET 2
SCALE 1:200



Scale	1:200
Sheet	A1
Project No.	2023-02-05
Draw No.	C1004
Rev.	D3

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

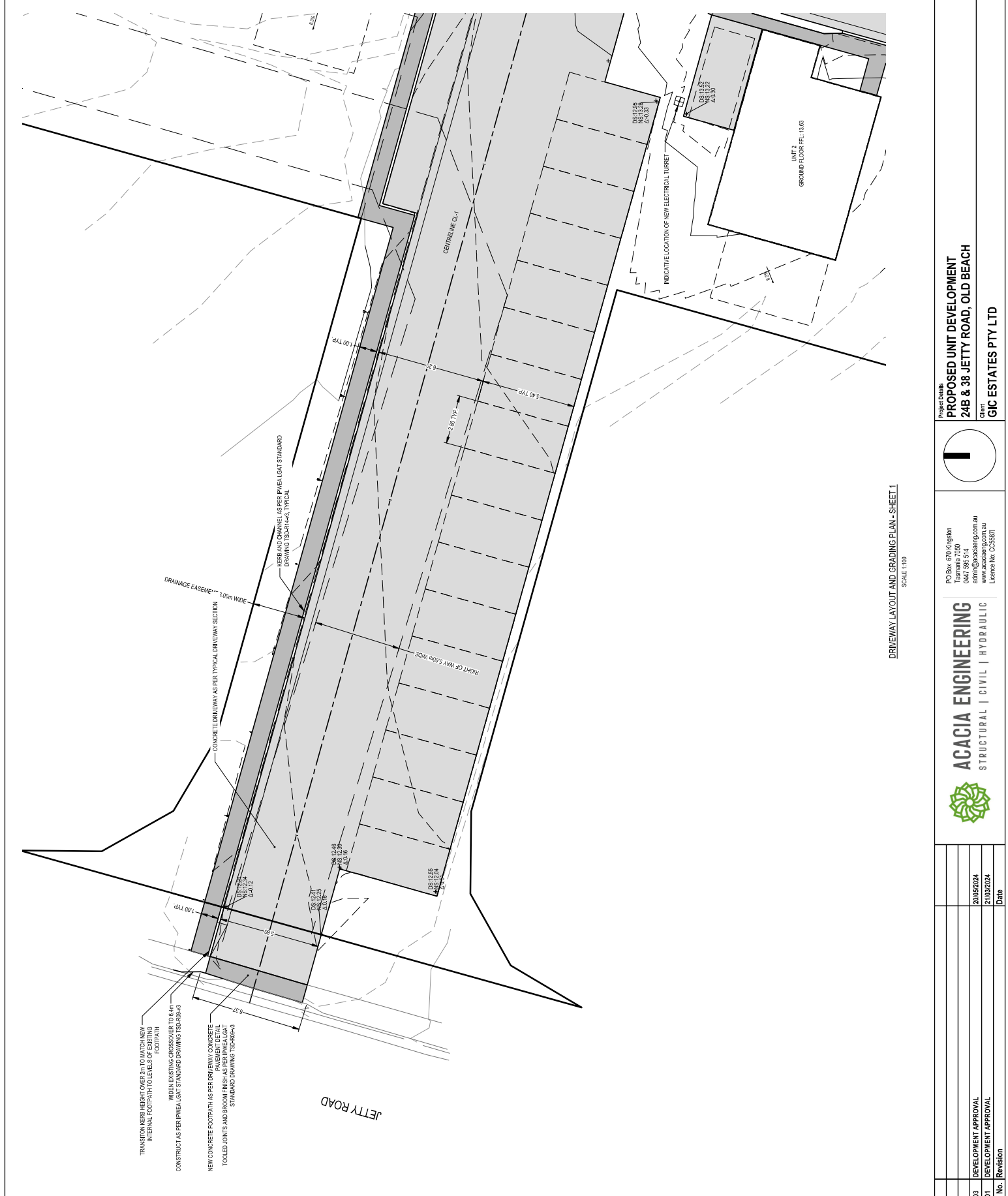
Client
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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024




TRANSITION KERB HEIGHT OVER 250 TO MATCH EXISTING INTERNAL FOOTPATH TO LEVELS OF EXISTING FOOTPATH
 WHEN EXISTING CROSSOVER TO 0.644 CONSTRUCT AS PER PAVED LGAT STANDARD DRAWING TSD-02-03
 NEW CONCRETE FOOTPATH AS PER DRIVEWAY CONCRETE PAVEMENT DETAIL TOOLED JOINTS AND BROOM FINISH AS PER STANDARD DRAWING TSD-02-03

KERB AND CHANNELS AS PER PAVED LGAT STANDARD DRAWING TSD-02-04, TYPICAL
 DRAINAGE EASEMENT 3.00m WIDE
 CONCRETE DRIVEWAY AS PER TYPICAL DRIVEWAY SECTION
 RIGHT OF WAY 5.00m WIDE
 CENTRELINE CL. 1
 INDICATIVE LOCATION OF NEW ELECTRICAL TURRET
 UNIT 2 GROUND FLOOR FFL 13.63

JETTY ROAD

DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 1
SCALE 1:100

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	Project Name: PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH Client: GIC ESTATES PTY LTD
No. D3 Description: DEVELOPMENT APPROVAL Date: 2016/2/24	No. D1 Description: DEVELOPMENT APPROVAL Date: 21/03/2024
No. D3 Description: DEVELOPMENT APPROVAL Date: 21/03/2024	No. D3 Description: DEVELOPMENT APPROVAL Date: 21/03/2024

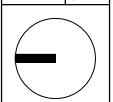


DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 2
SCALE 1:100

No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	01/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024

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Project Name: **PROPOSED UNIT DEVELOPMENT**
 24B & 38 JETTY ROAD, OLD BEACH
 Client: **GIC ESTATES PTY LTD**

No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	01/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024




DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 3
SCALE 1:100

Project Name	DRIVEWAY LAYOUT AND GRADING PLAN		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C2003 D1

Project Name
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH

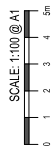
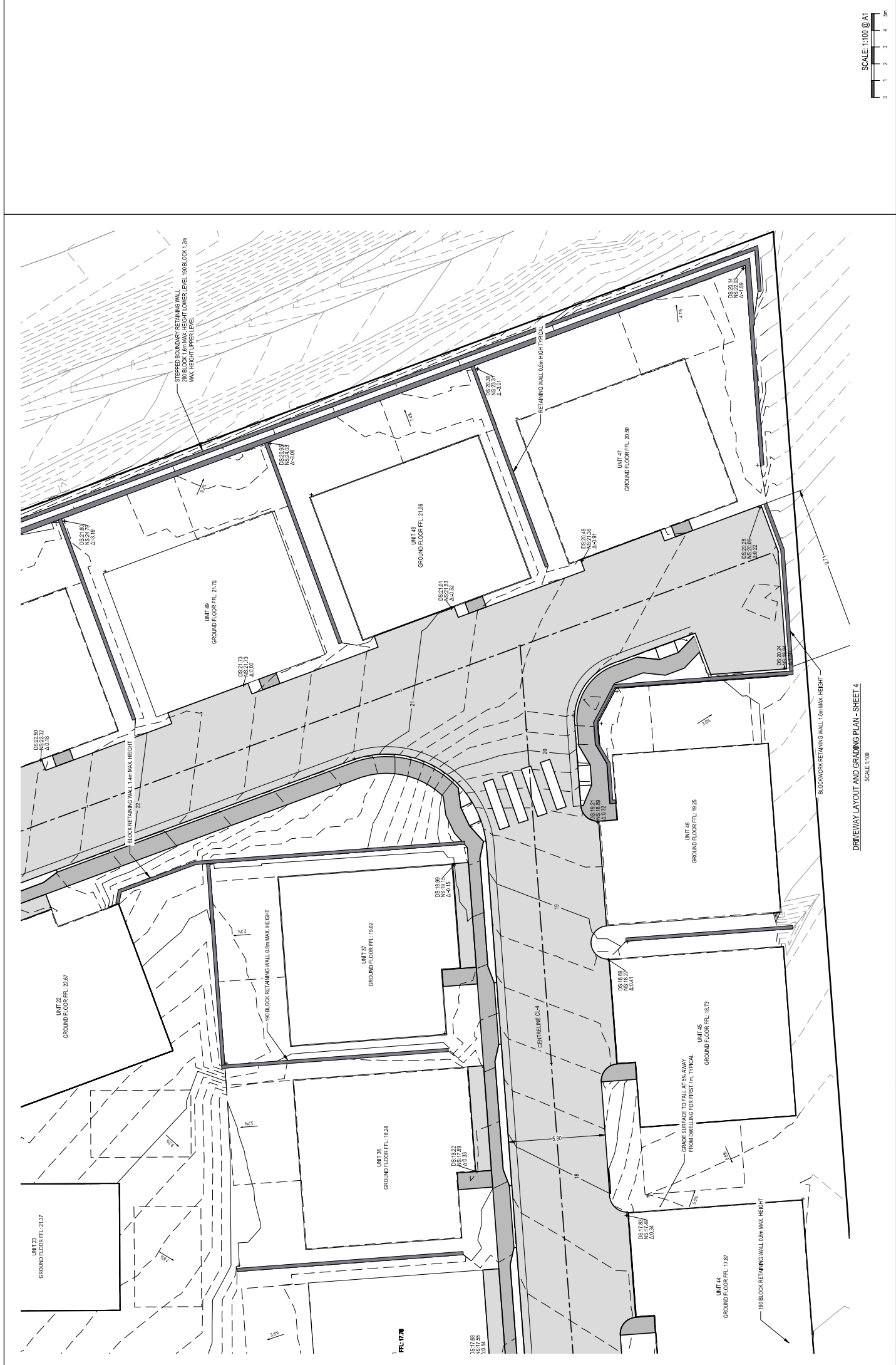
Client
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



Project Name	DRIVEWAY LAYOUT AND GRADING PLAN		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C2004_D1

Project Name
PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH

Client
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No.	Revision
D1	DEVELOPMENT APPROVAL
	21032024
	Pa6

DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 4
 SCALE 1:100

FFL 17.78

DS 17.68
 NS 17.55
 A 17.44

DS 17.80
 NS 17.67
 A 17.54

UNIT 44
 GROUND FLOOR FFL 17.27

UNIT 45
 GROUND FLOOR FFL 18.73

UNIT 46
 GROUND FLOOR FFL 19.25

DS 18.27
 NS 18.25
 A 18.32

UNIT 47
 GROUND FLOOR FFL 20.30

UNIT 48
 GROUND FLOOR FFL 21.08

UNIT 49
 GROUND FLOOR FFL 21.78

UNIT 22
 GROUND FLOOR FFL 22.67

UNIT 23
 GROUND FLOOR FFL 21.37

STEPS DOWNWAY / RETAINING WALL
 LOWER LEVEL, 198 BLOCK, 1.2m
 MAX. HEIGHT UPPER LEVEL.

BLOCK RETAINING WALL 1.4m MAX HEIGHT

198 BLOCK RETAINING WALL 0.8m MAX HEIGHT

RETAINING WALL 0.6m HIGH TYPICAL

BLOCKWORK RETAINING WALL 1.0m MAX HEIGHT

GRADE TO BE 10% FLAT 5% AWAY
 FROM LINE USING FORM FIRST 1m TYPICAL

CENTRELINE G1.4

DS 19.27
 NS 19.26
 A 19.35

UNIT 36
 GROUND FLOOR FFL 18.28

UNIT 37
 GROUND FLOOR FFL 19.02

DS 18.99
 NS 18.97
 A 19.15

DS 21.73
 NS 21.73
 A 21.073

DS 21.60
 NS 21.71
 A 21.15

DS 20.95
 NS 21.07
 A 20.98

DS 20.39
 NS 20.37
 A 20.07

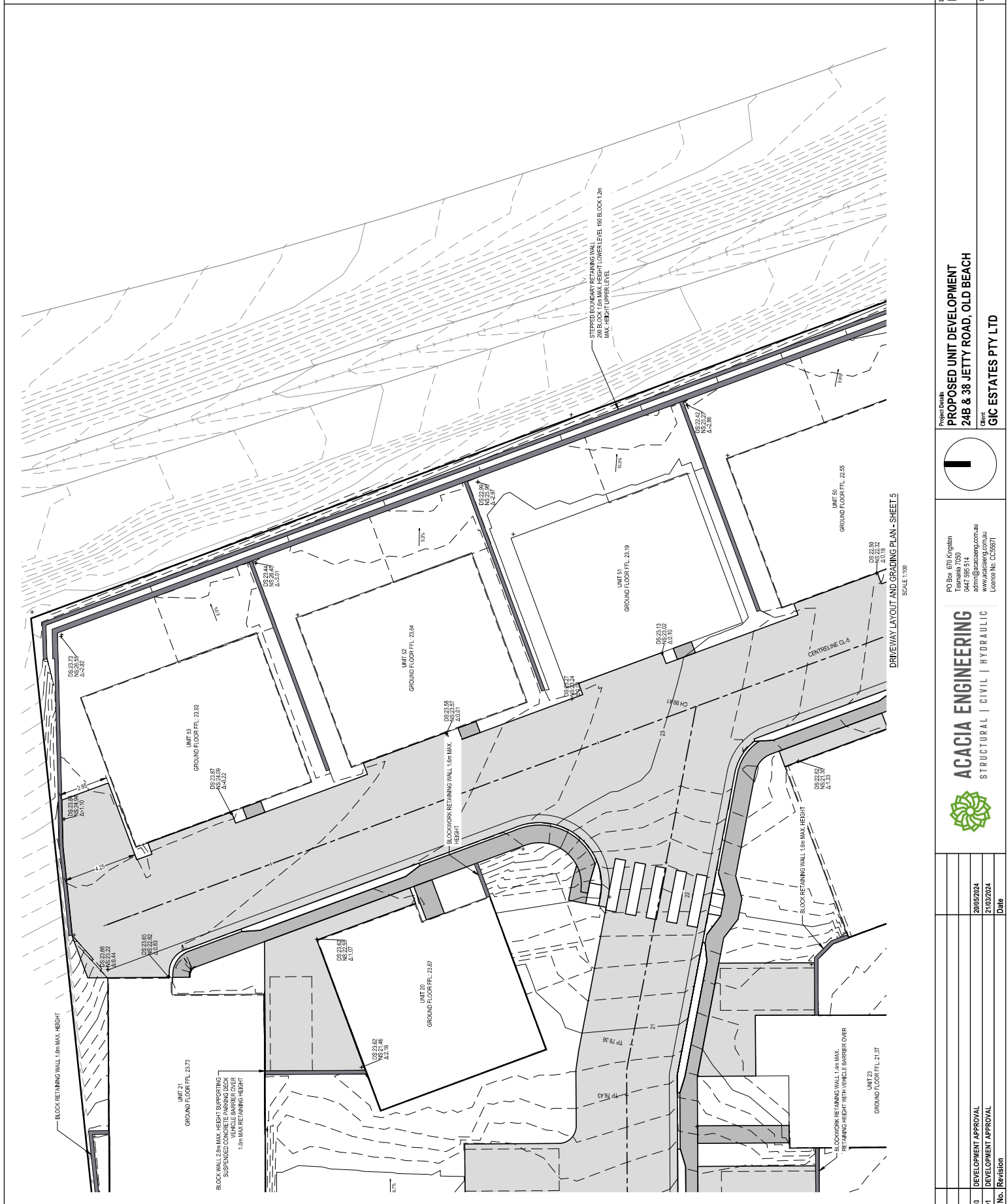
DS 20.46
 NS 21.06
 D 20.57

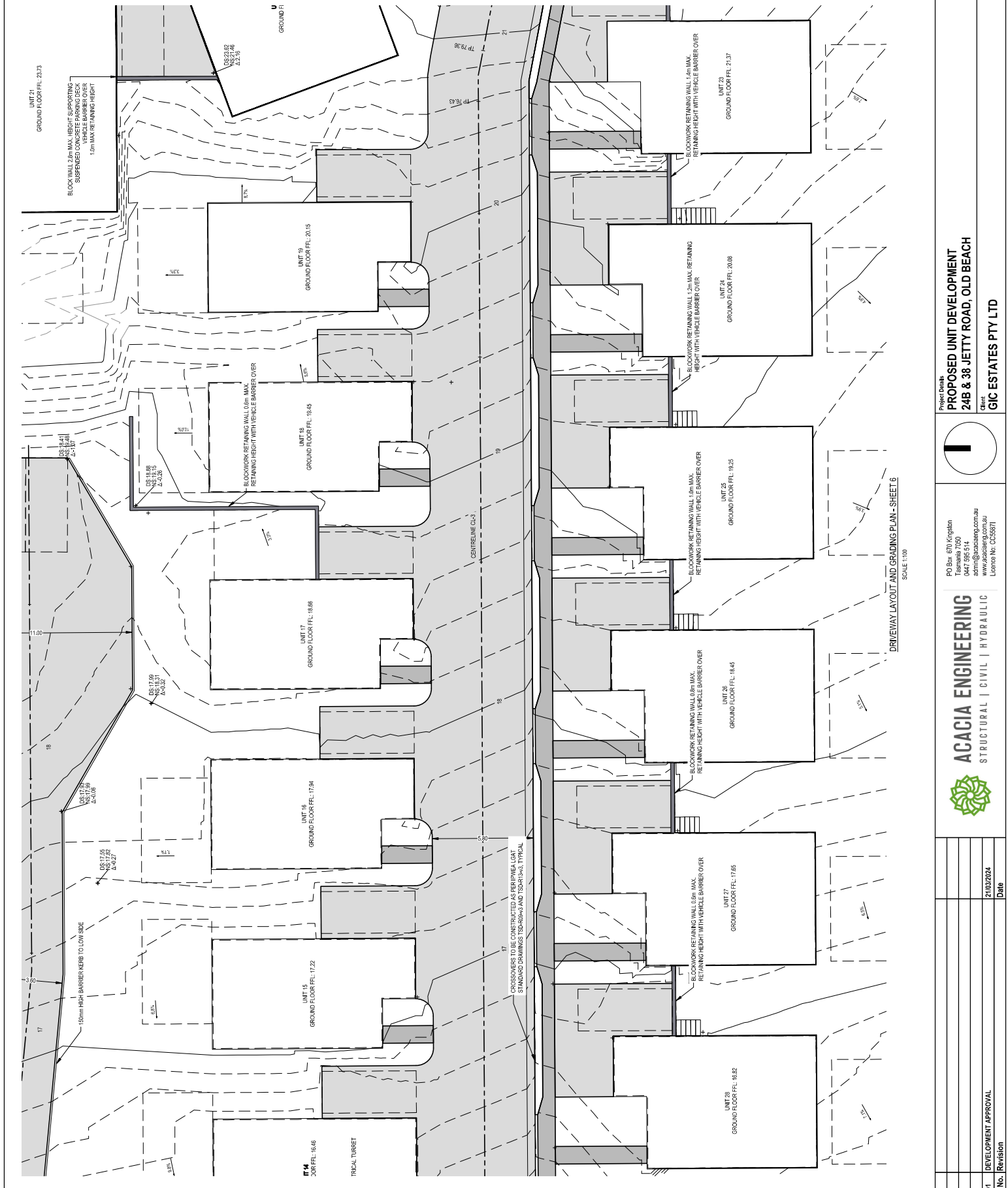
DS 20.78
 NS 20.78
 A 20.22

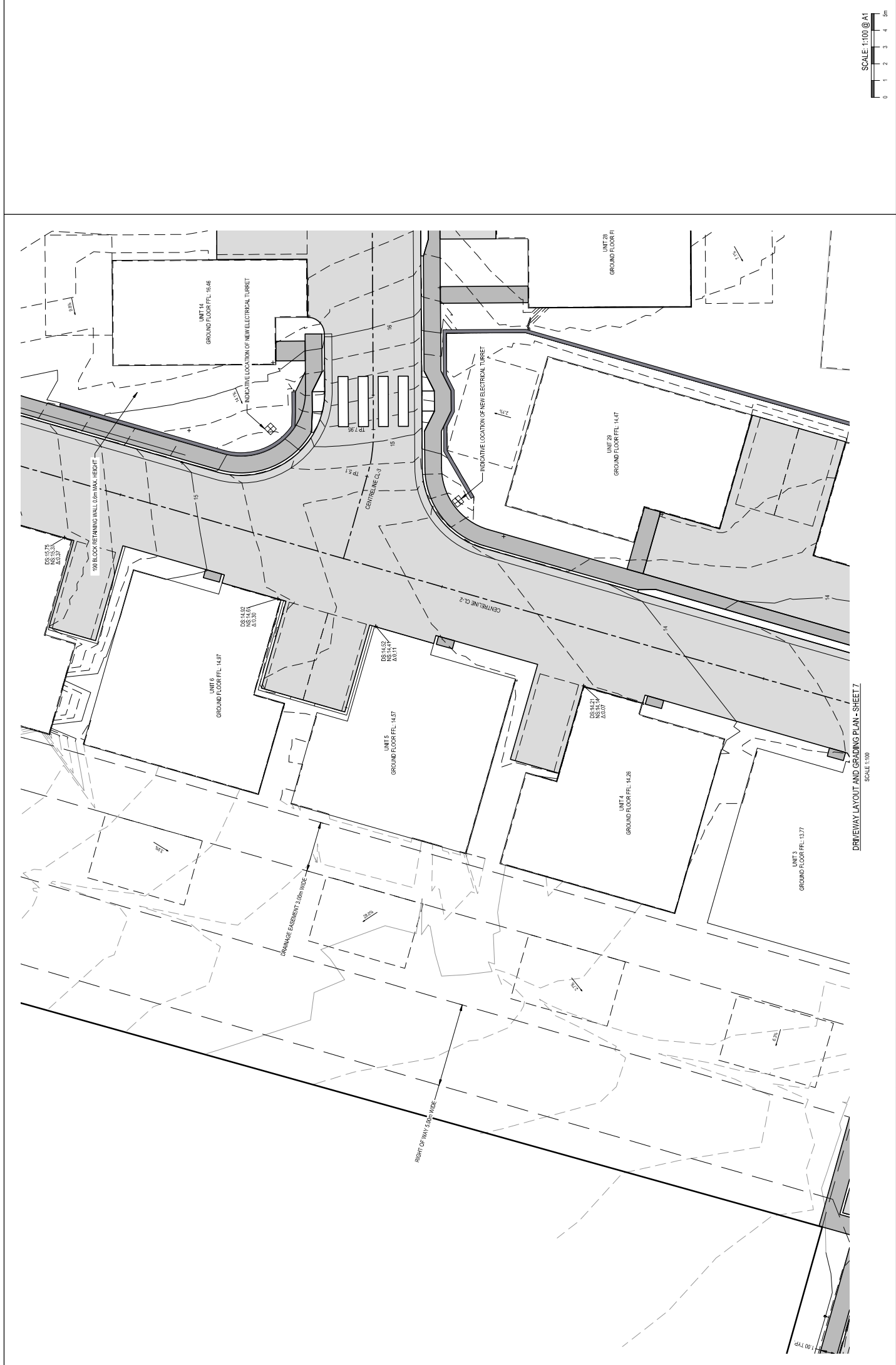
DS 20.14
 NS 22.05
 A 20.25

DS 20.24
 NS 20.24
 A 20.24

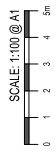








DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 7
SCALE 1:100

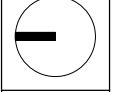


Drawing Name	DRIVEWAY LAYOUT AND GRADING PLAN		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C2007 D3

No.	Revision
D3	DEVELOPMENT APPROVAL
D1	DEVELOPMENT APPROVAL

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

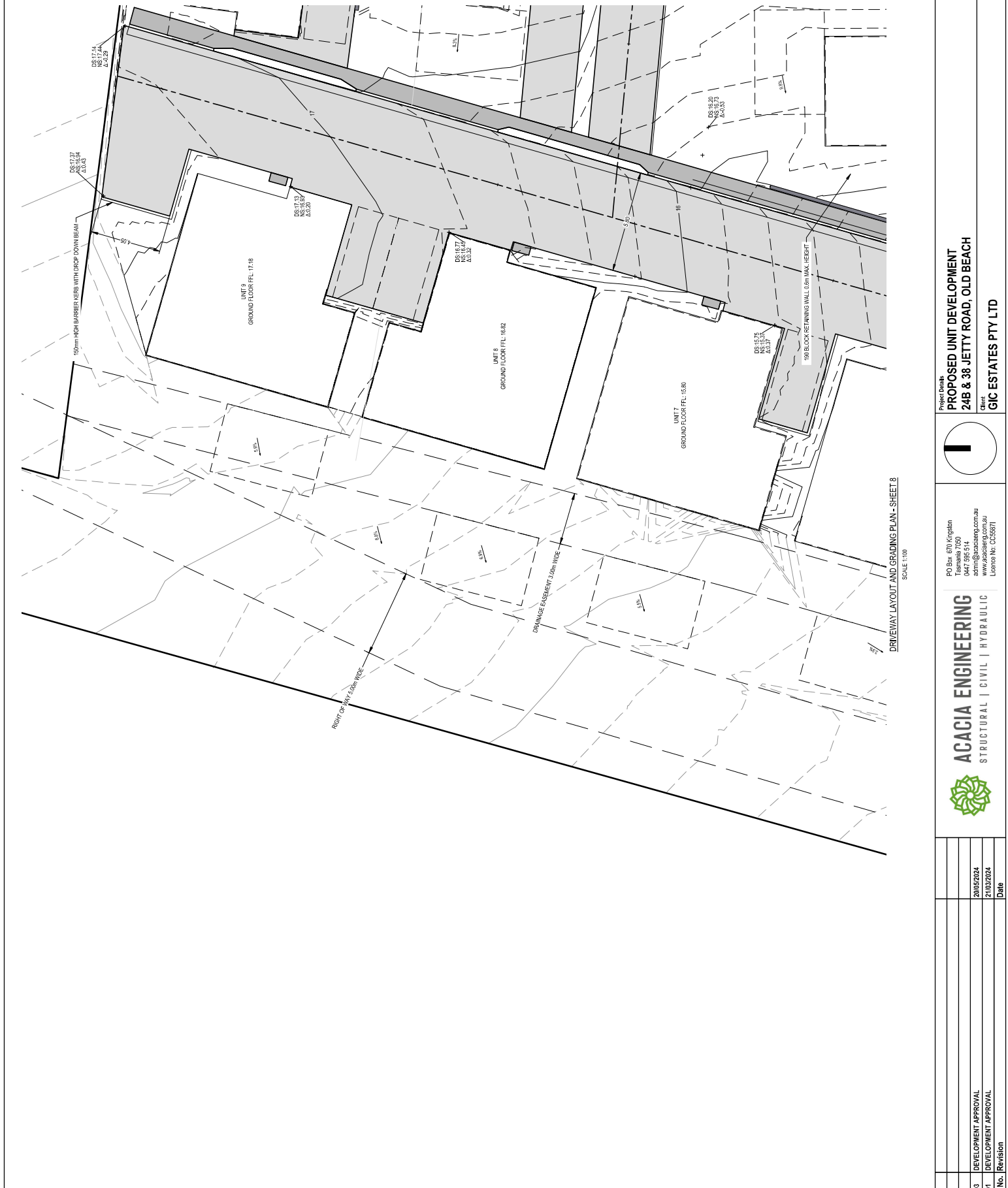
Client
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Date	2015/02/24
Date	21/03/2024



No.	Revision	Date
D3	DEVELOPMENT APPROVAL	2005/2024
D1	DEVELOPMENT APPROVAL	21/03/2024

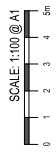
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Project Details
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH
 Client
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DRIVEWAY LAYOUT AND GRADING PLAN - SHEET 9
SCALE 1:100



Project Name	DRIVEWAY LAYOUT AND GRADING PLAN		
Scale	1:100	Sheet No.	A1
Project No.	2023-02-05	Draw No.	C2009
Rev.	D1	Rev.	D1

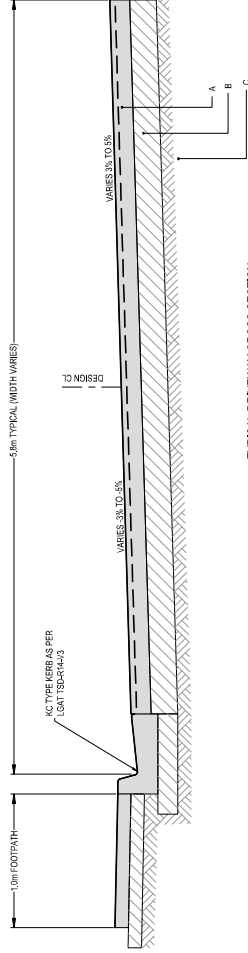
Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Client
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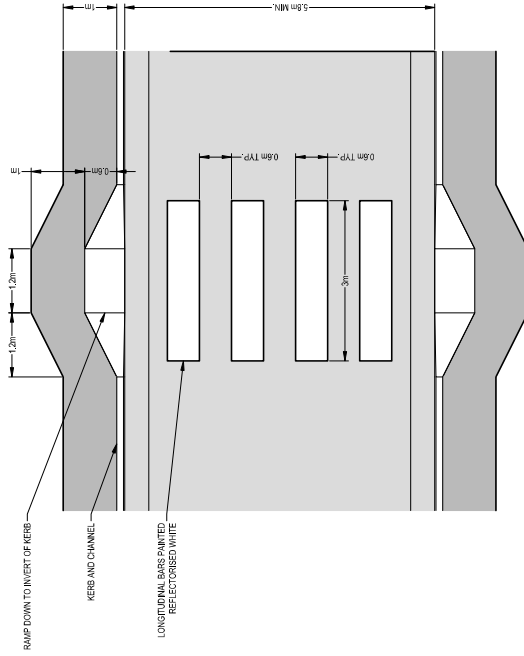
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024




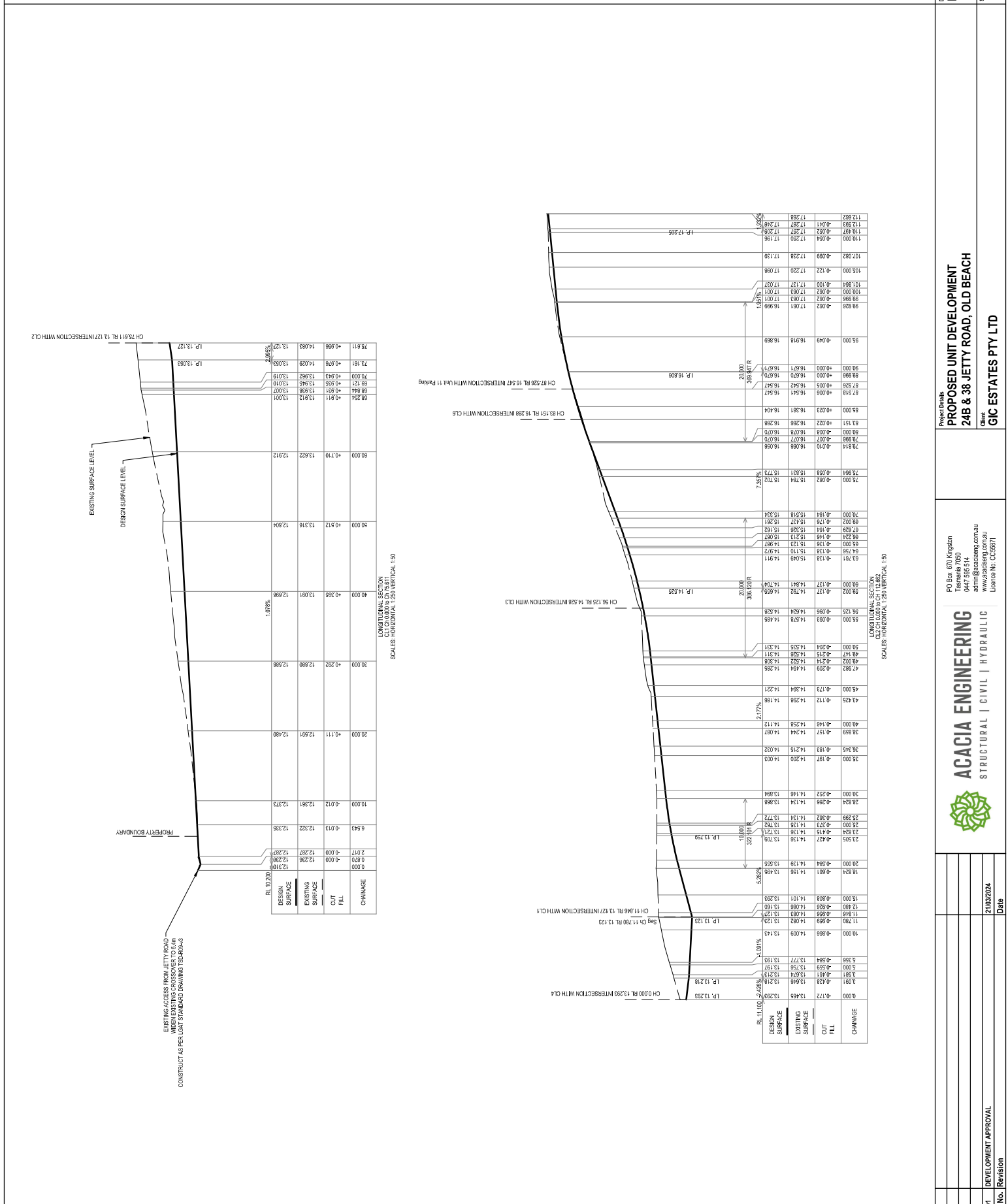
TYPICAL DRIVEWAY CROSS SECTION
SCALE 1:20

CONCRETE DRIVEWAY PAVEMENT		MATERIAL
PAVEMENT LAYER	THICKNESS (mm)	
A CONCRETE BASE COURSE	150	NZ2 CONCRETE WITH SUBMESH TOP (40mm MML COVER) IN SITU MATERIAL CBR ≥ 2%
B CONCRETE SURFACE COURSE	40	
C SUBGRADE		



TYPICAL PEDESTRIAN CROSSING LAYOUT
SCALE 1:50

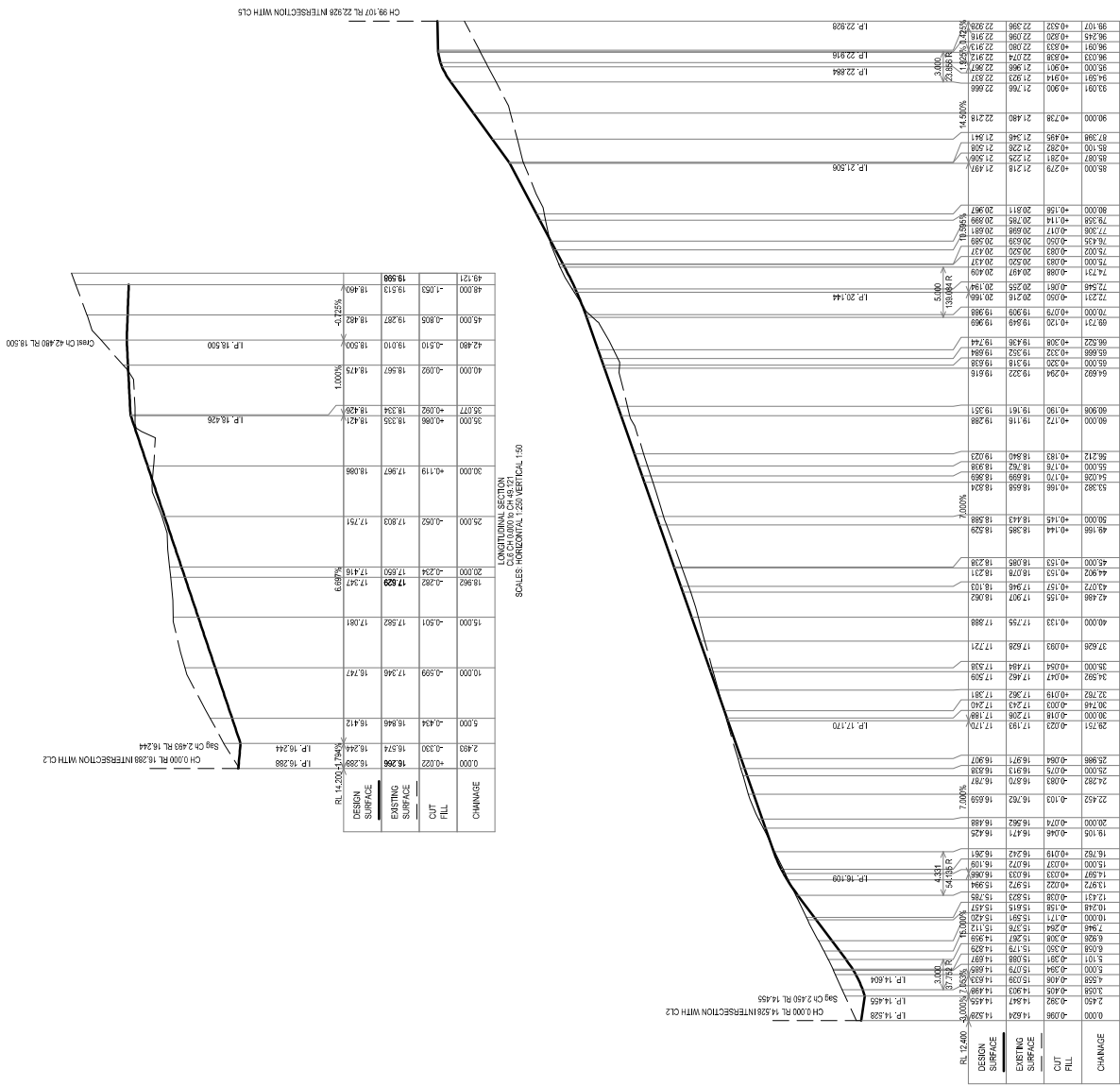
 ACACIA ENGINEERING STRUCTURAL CIVIL HYDRAULIC		Project Details PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH		Drawing Name DRIVEWAY TYPICAL DETAILS	
PO Box 670 Wacol Queensland 4050 0447 595 514 admin@acacieng.com.au www.acacieng.com.au License No. CC5891		Project No. 2023-02-05		Date No. C2101	
No. Revision D1		Scale A1		Rev. D1	



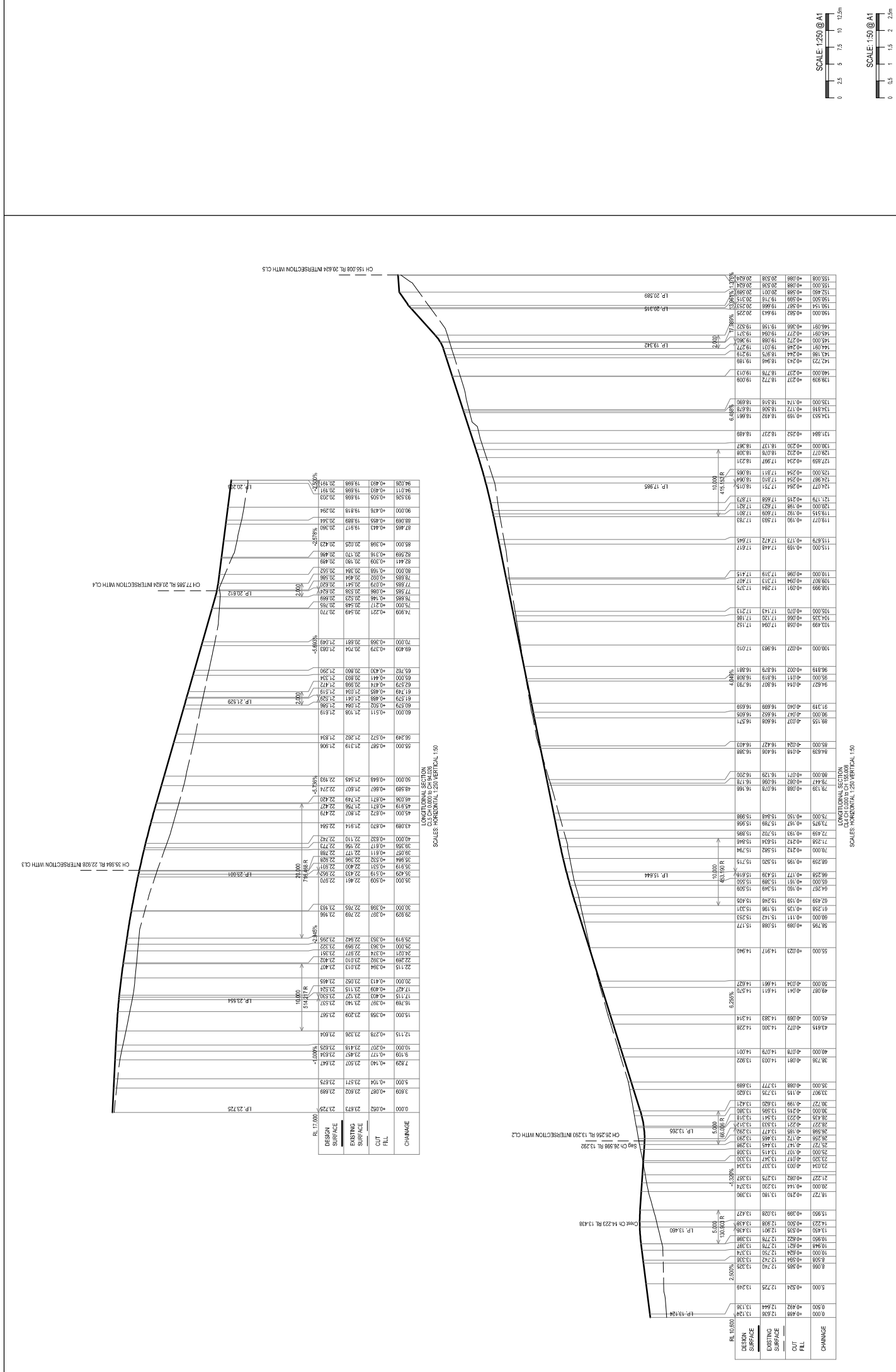
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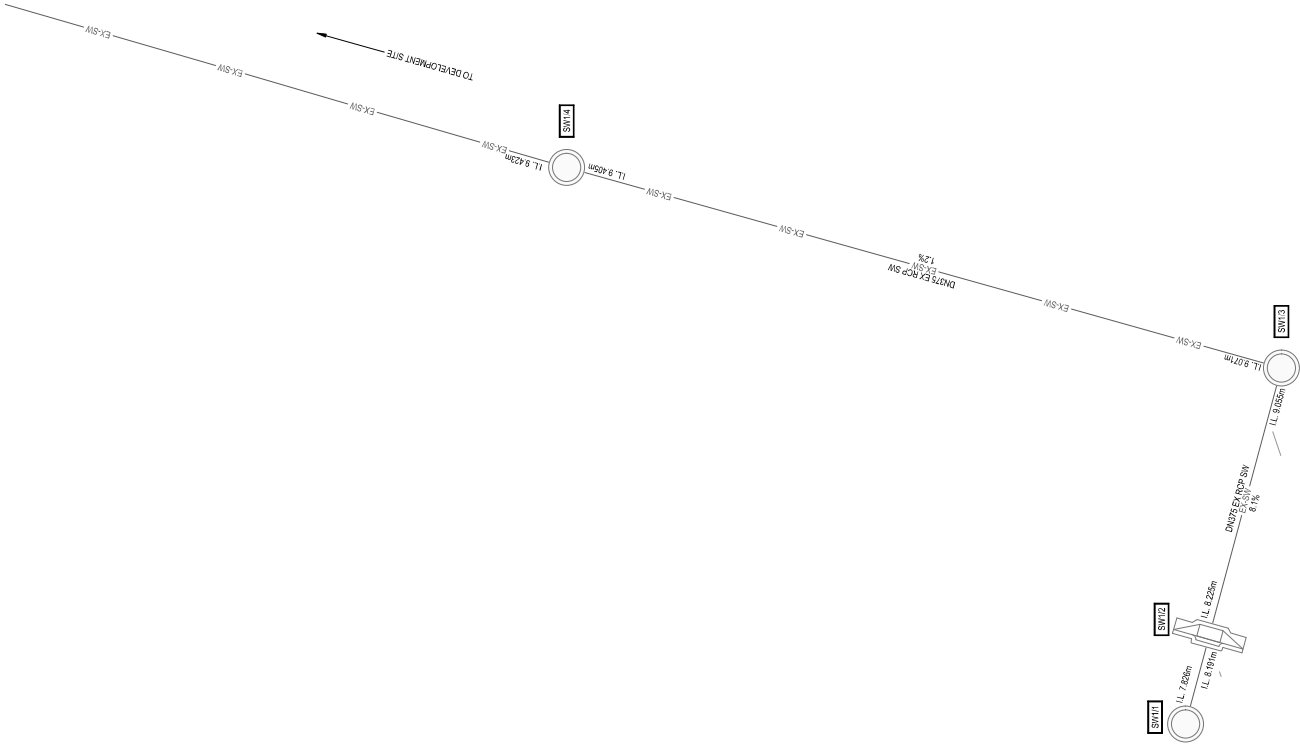
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Project Name: **PROPOSED UNIT DEVELOPMENT**
24B & 38 JETTY ROAD, OLD BEACH
 Sheet: **DR1**
 Scale: **1:100**
 Project No: **2023-02-05**
 Date: **21/03/2024**
 Client: **GIC ESTATES PTY LTD**

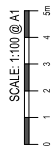




No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024

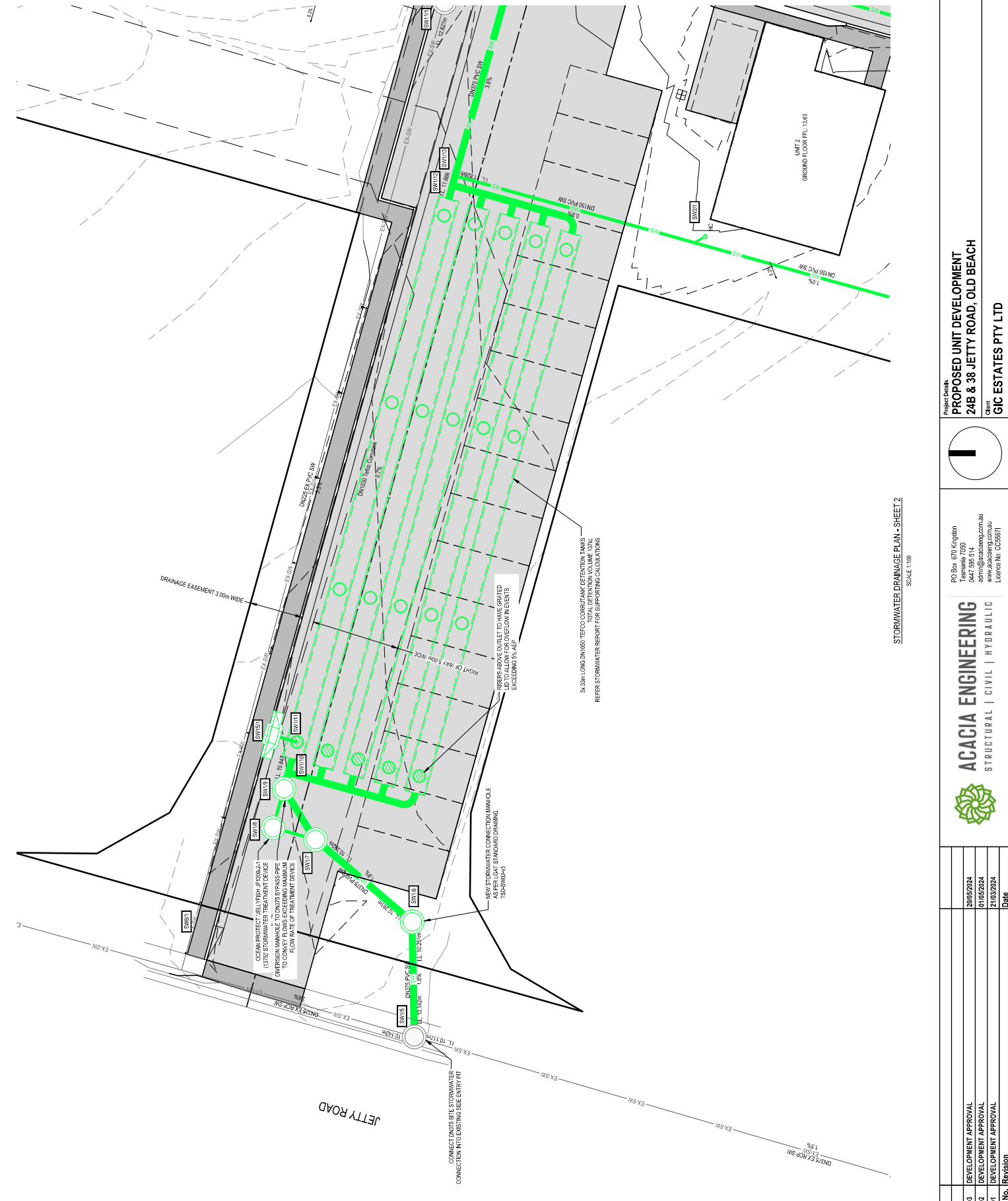





STORMWATER DRAINAGE PLAN - SHEET 1
SCALE 1:100



 <p>ACACIA ENGINEERING STRUCTURAL CIVIL HYDRAULIC</p>	<p>PO Box 670 McClellan Tasmania 7050 0447 995 514 admin@acacieng.com.au www.acacieng.com.au Licence No. CC5581</p>		<p>Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH</p>	<p>Drawing Name STORMWATER DRAINAGE PLAN - SHEET 1</p>
	<p>No. Revision</p> <p>D1 DEVELOPMENT APPROVAL</p> <p>21/03/2024</p> <p>Date</p>	<p>Scale 1:100</p>	<p>Sheet A1</p>	<p>Project No. 2023-02-05</p>




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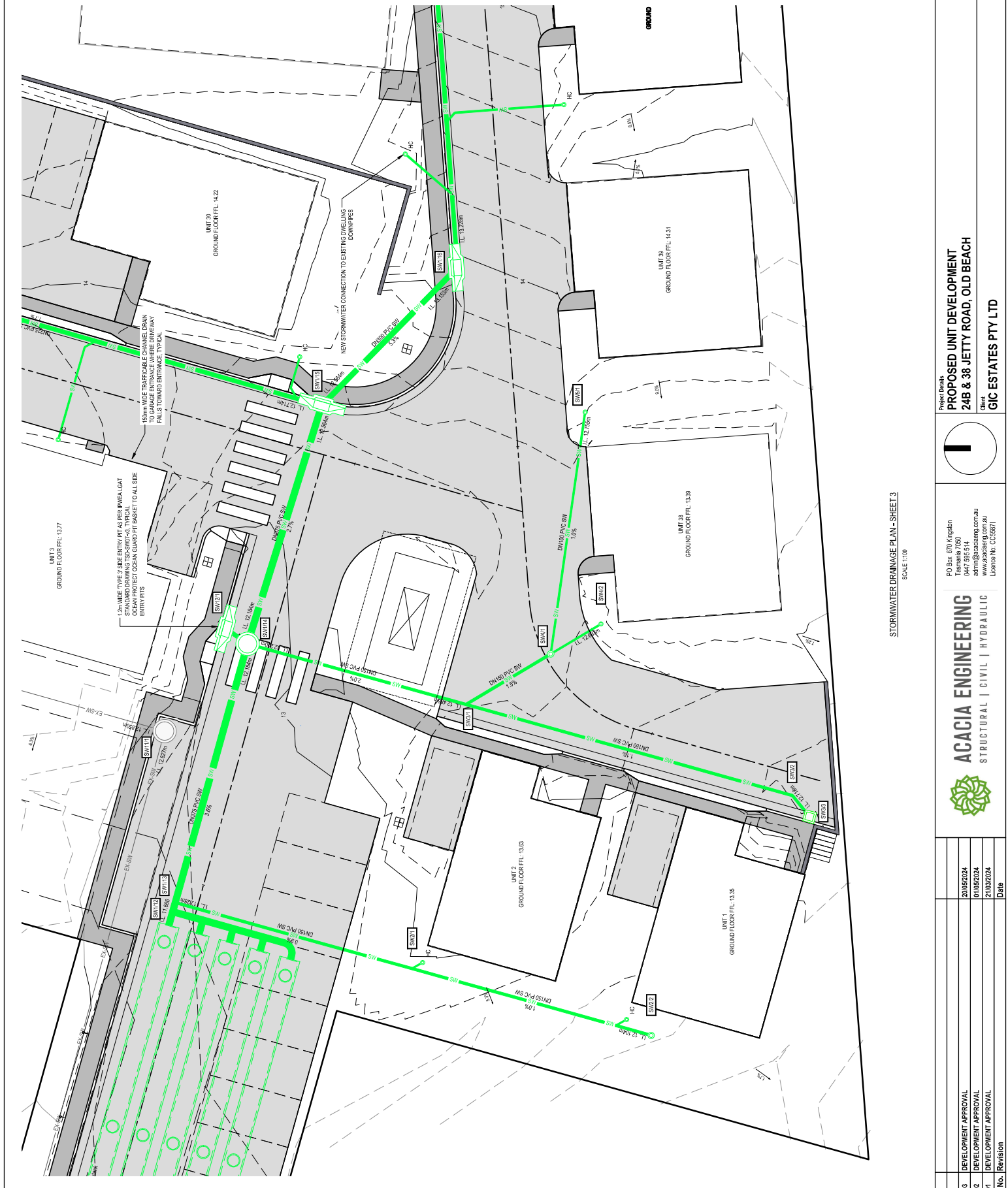
Project Details:
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH

Client:
GIC ESTATES PTY LTD

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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	01/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024

STORMWATER DRAINAGE PLAN - SHEET 2
 SCALE 1:100

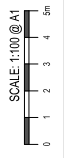


STORMWATER DRAINAGE PLAN - SHEET 3
 SCALE 1:100

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PROJECT DATA PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH		CLIENT GIC ESTATES PTY LTD	
NO.	REVISION	DATE	APPROVAL
D3	DEVELOPMENT APPROVAL	20/03/2024	[Signature]
D2	DEVELOPMENT APPROVAL	01/03/2024	[Signature]
D1	DEVELOPMENT APPROVAL	21/03/2024	[Signature]



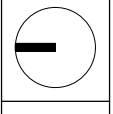
STORMWATER DRAINAGE PLAN - SHEET 4
SCALE 1:100



Project Name	STORMWATER DRAINAGE PLAN - SHEET 4		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C3004 D1

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

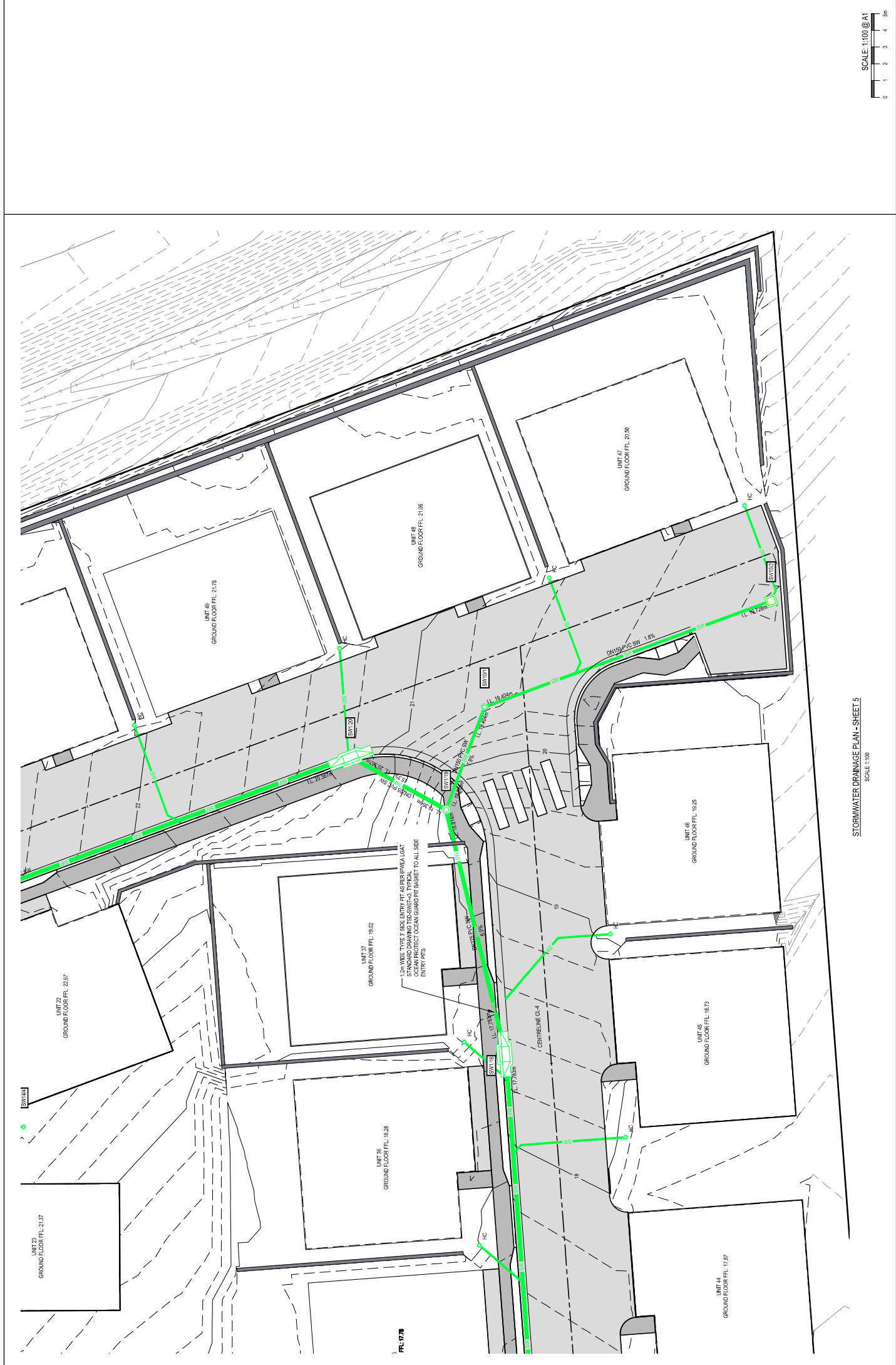
Client
GIC ESTATES PTY LTD



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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



Scale	1:100	Sheet	A1	Project No.	2023-02-05	Draw No.	C3005	Rev.	D1
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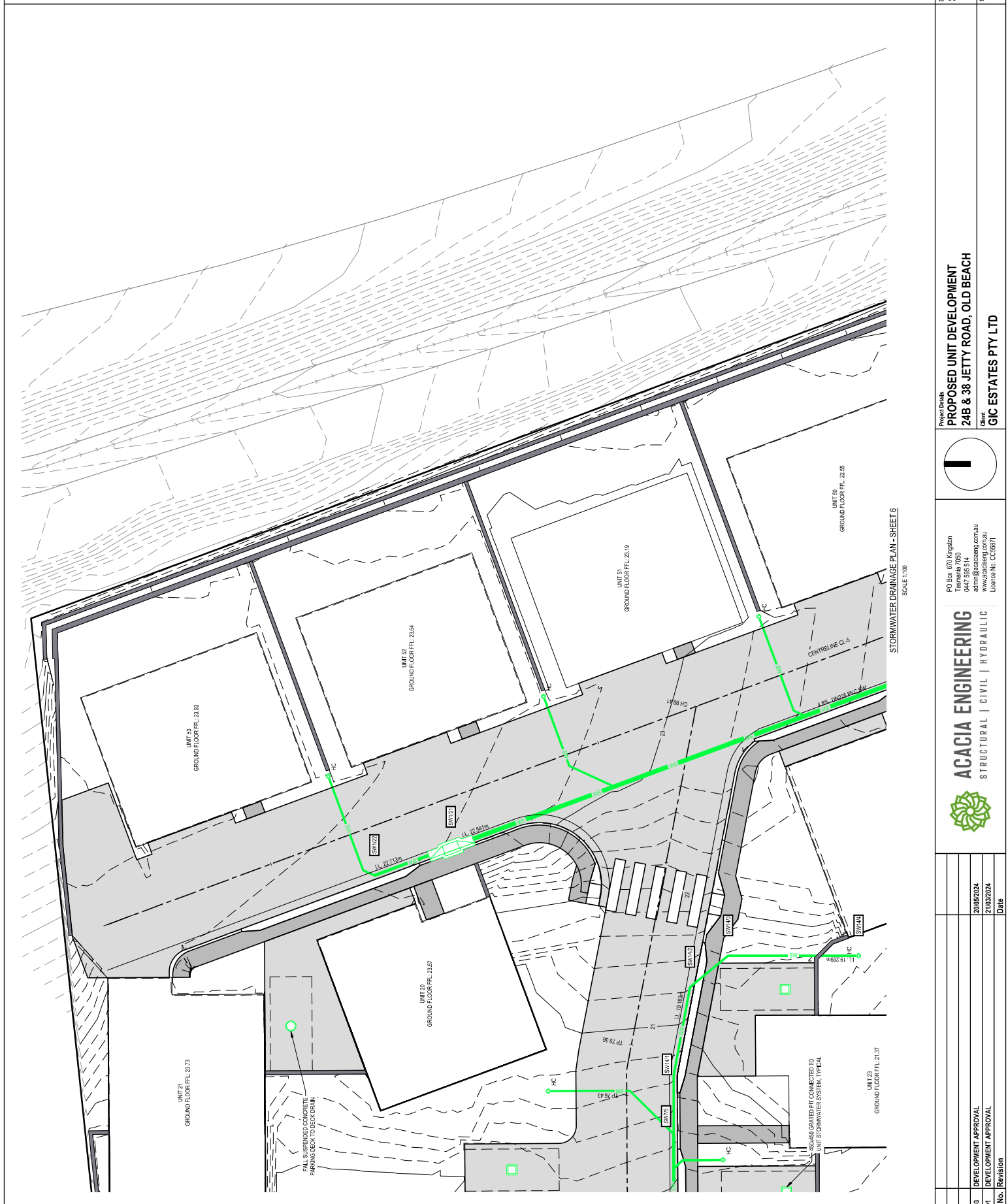
STORMWATER DRAINAGE PLAN - SHEET 5
SCALE 1:100



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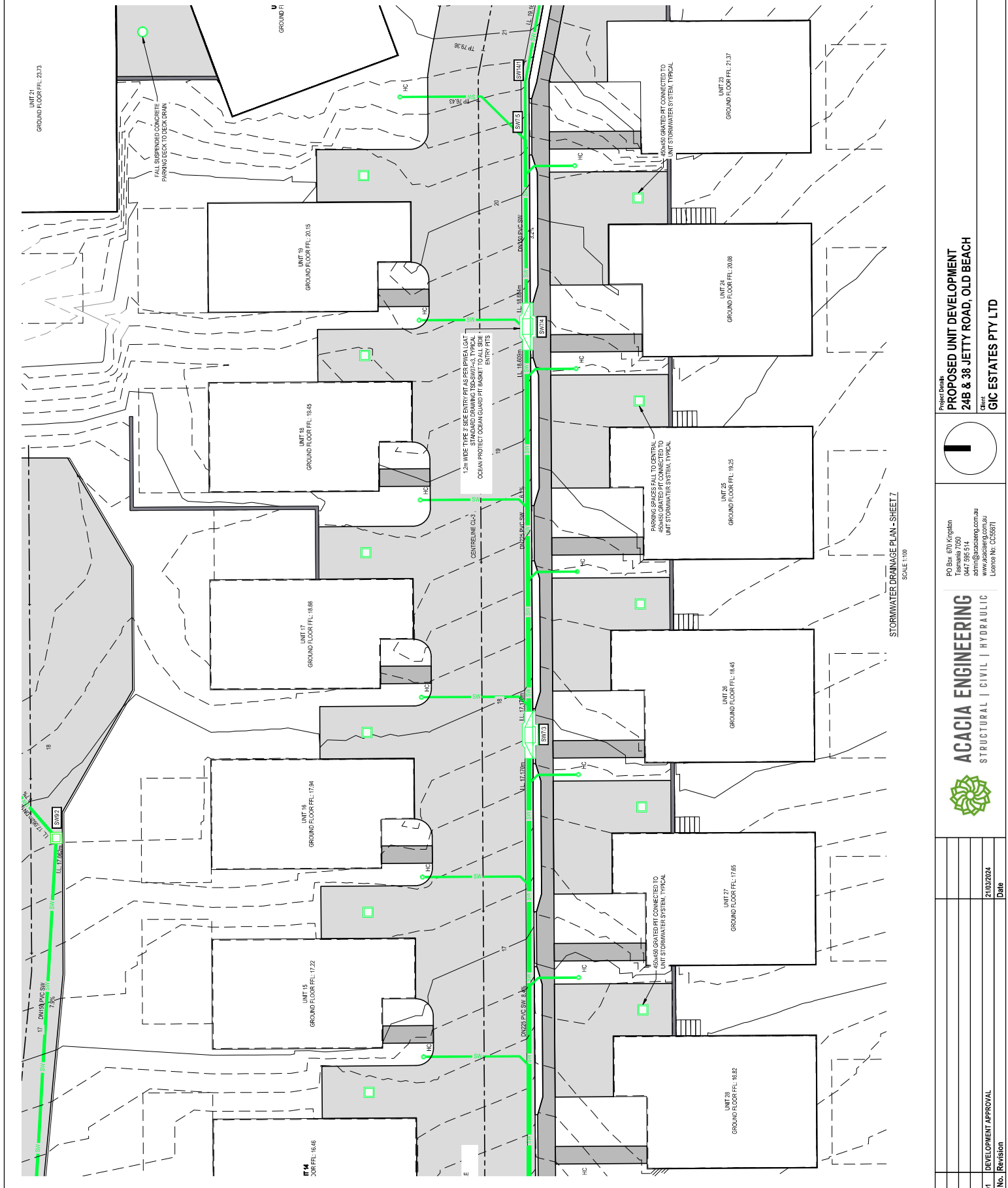
PO Box 670 Wagon
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Project No. **2023-02-05**
Sheet **A1**
Drawing Name **STORMWATER DRAINAGE PLAN - SHEET 5**

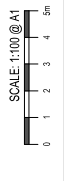
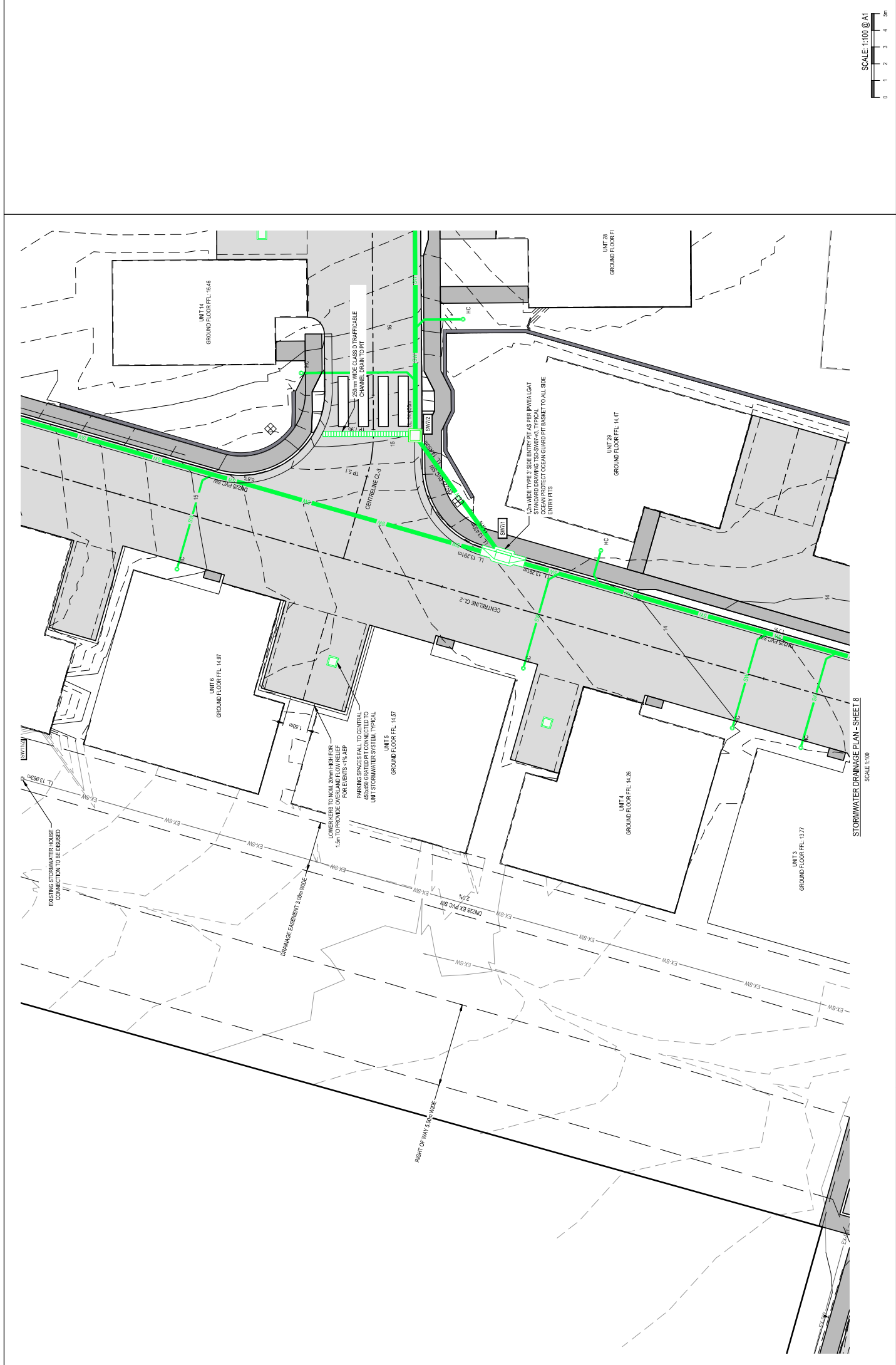
No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



		Acacia Engineering STRUCTURAL CIVIL HYDRAULIC	PO Box 670 Jackson Taranaki 7550 0447 995 614 admin@acaciang.com.au Licence No. CC5891
		Project Name: PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH	



No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



Drawing Name:		STORMWATER DRAINAGE PLAN - SHEET 8	
Scale:	A1	Project No.:	2023-02-05
Sheet:	1:100	Draw No.:	C3008_D3

Project Name:
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH

Client:
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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	2016/02/24
D1	DEVELOPMENT APPROVAL	21/03/2024



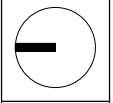
STORMWATER DRAINAGE PLAN - SHEET 9
SCALE 1:100



Drawing Name	STORMWATER DRAINAGE PLAN - SHEET 9		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C3009 D3

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

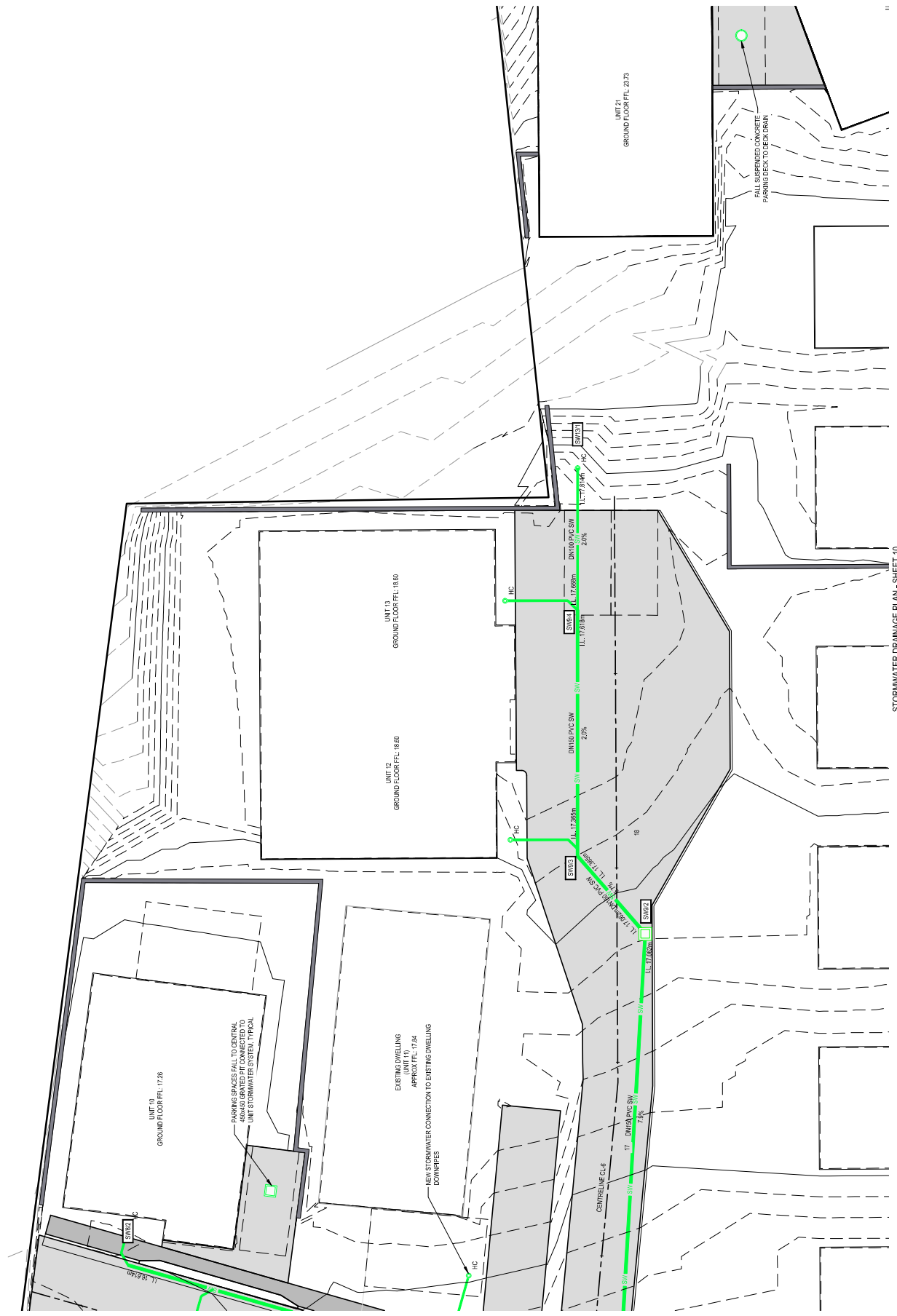
Client
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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D1	DEVELOPMENT APPROVAL	27/03/2024



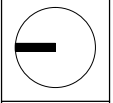
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SCALE 1:100

SCALE: 1:100 @ A1
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Drawing Name	STORMWATER DRAINAGE PLAN - SHEET 10		
Scale	A1	Project No.	2023-02-05
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		Rev.	D1

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

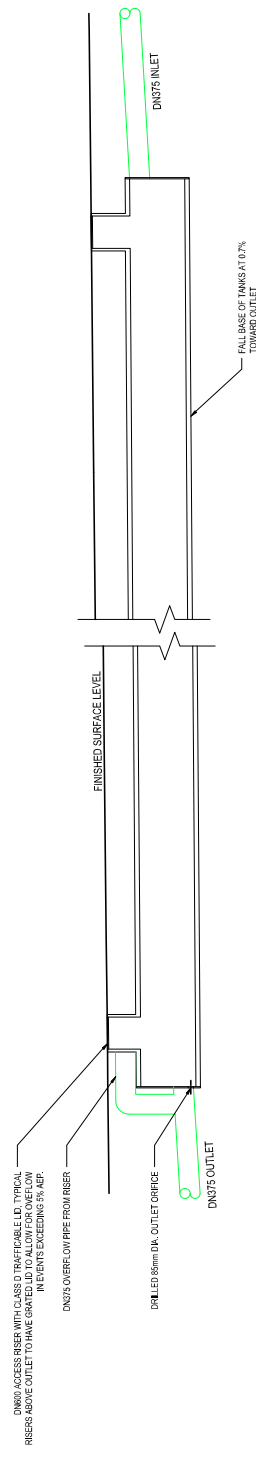
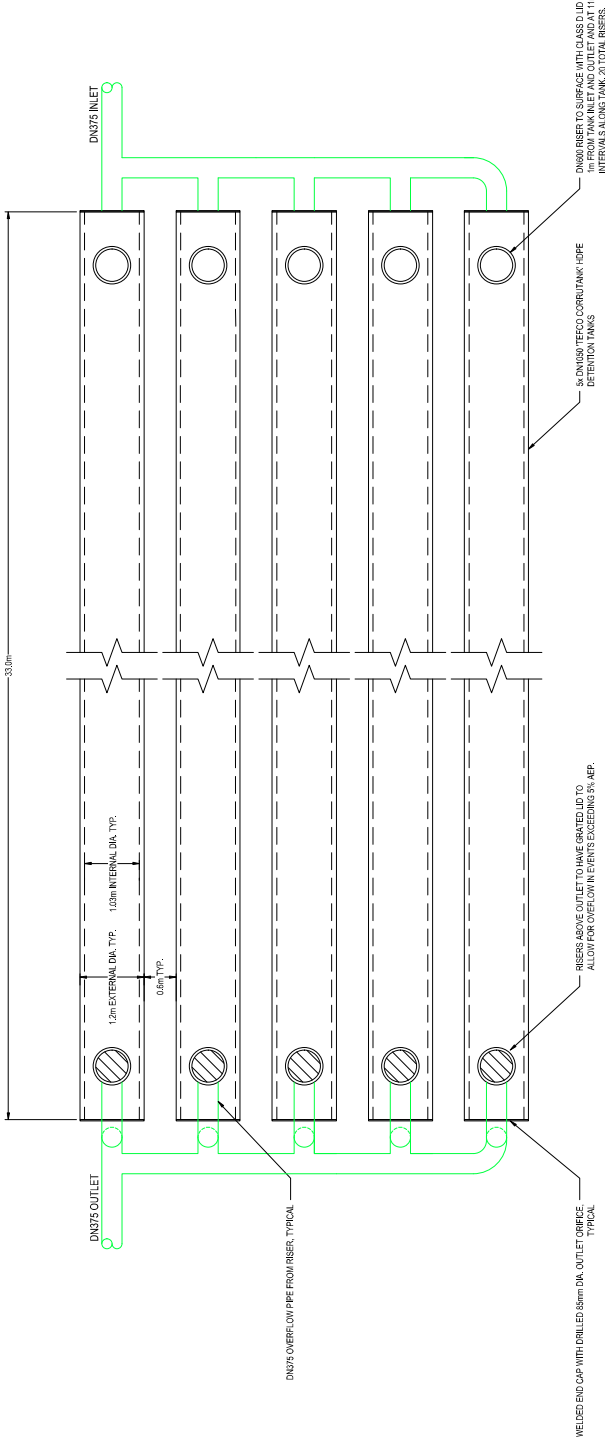
Client
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



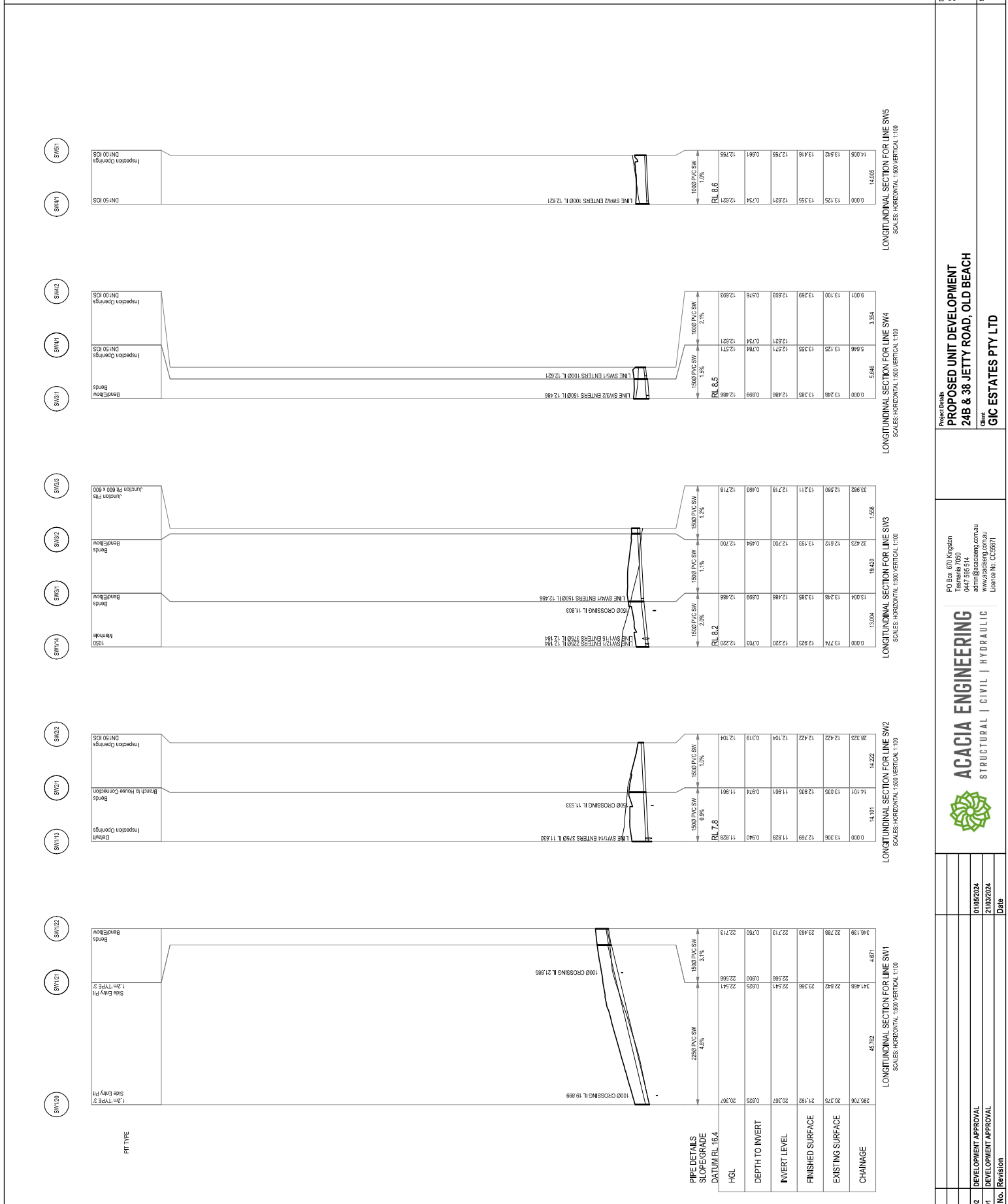
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Project No.	2023-02-05	Draw No.	C3101
Rev.		Rev.	D2

Project Details
PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH
 Client
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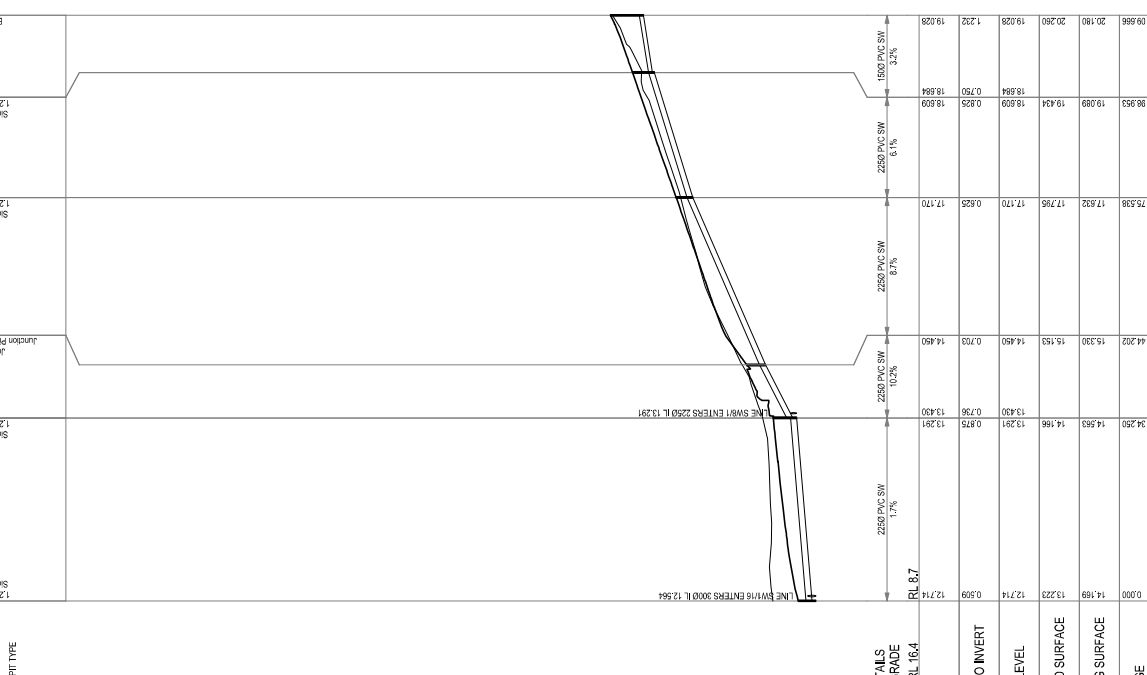
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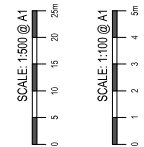
No.	Date
D2	01/05/2024
D1	21/03/2024
DEVELOPMENT APPROVAL	
DEVELOPMENT APPROVAL	



SW115 SW114 SW113 SW112 SW111



PIT TYPE



Drawing Name:
STORMWATER LONG SECTIONS - SHEET 3

Scale:
-

Sheet No:
A1

Project No:
2023-02-05

Drawn By:
C3203

Checked By:
D1

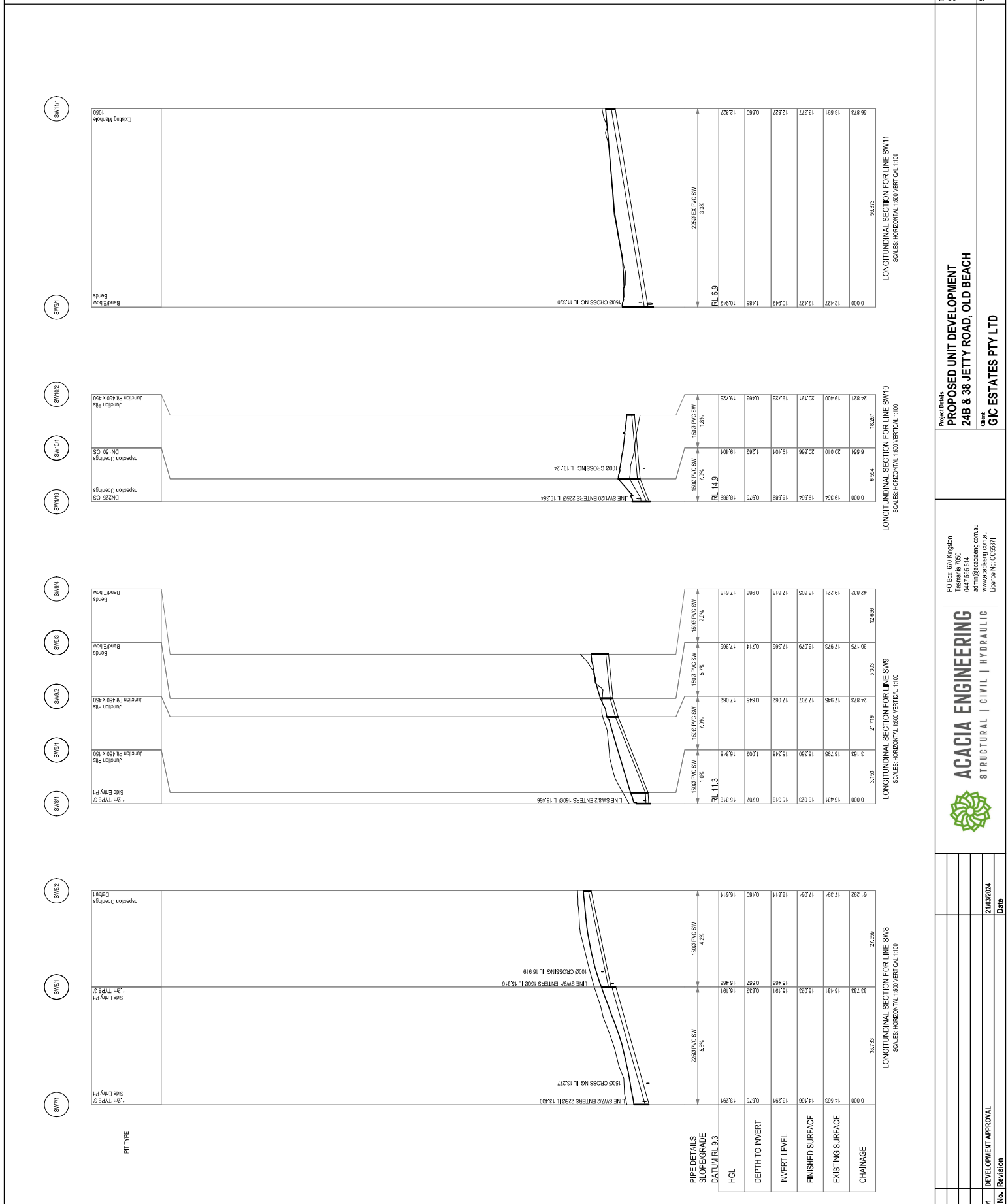
Project Details:
PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH

Client:
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



WATER LOADING CALCULATIONS

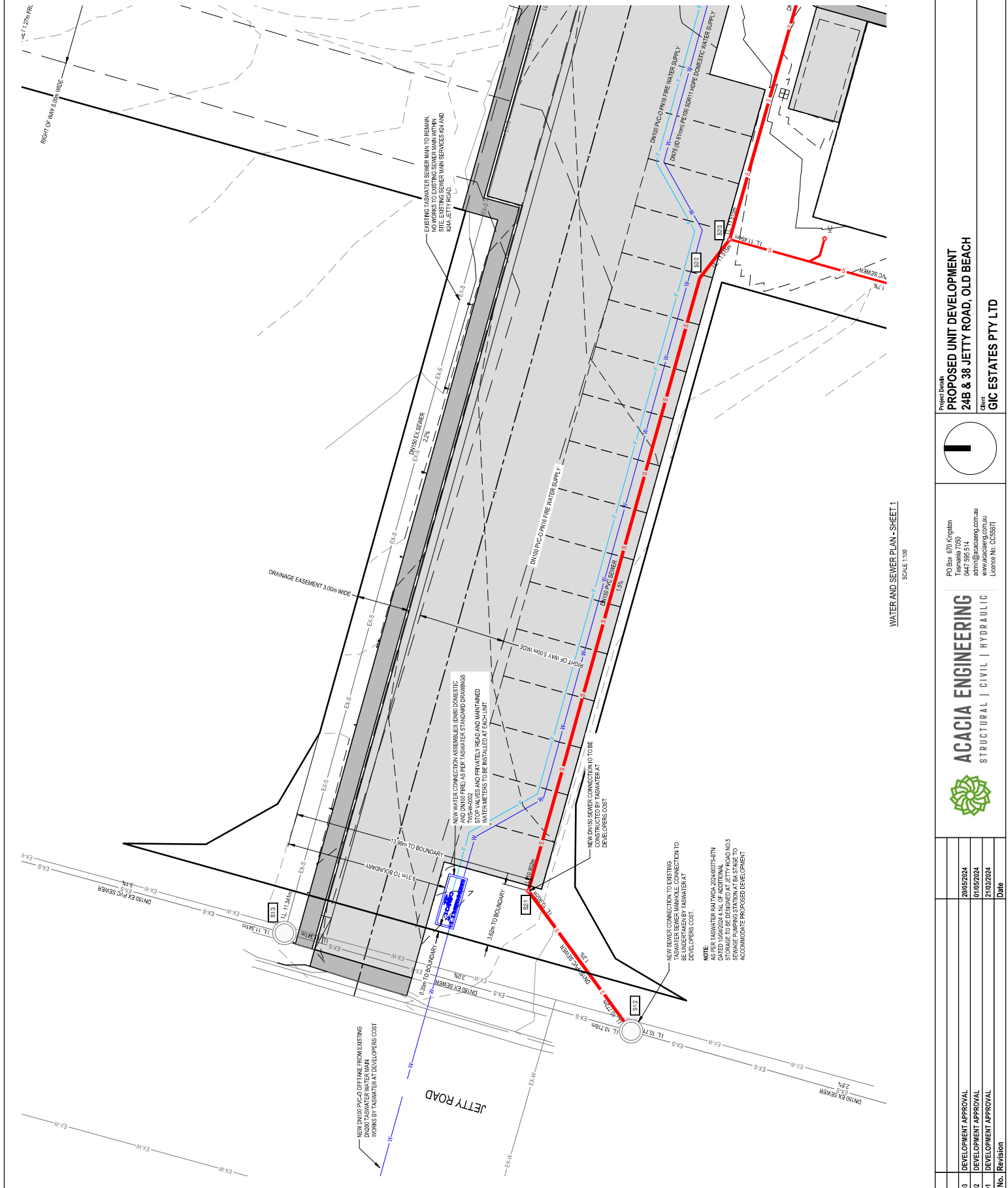
LOADINGS			
CODE	NO. OF UNITS	ET PER UNIT	ET
RM02	15	0.6	9
RM03	38	0.8	30.4
TOTAL	53	-	39.4

DEMAND		
ET	39.4	
AD	0.31 L/s	
PD	0.70 L/s	
PH	1.68 L/s	
PSD	4.90 L/s	
BASE DEMAND	1.89 L/s	
FIRE DEMAND	10.0 L/s	
PEAK	4.96 L/s	
20 PEAK + FIRE	13.27 L/s	

SEWERAGE LOADING CALCULATIONS

LOADINGS			
CODE	NO. OF UNITS	ET PER UNIT	ET
RM02	15	0.75	11.25
RM03	38	1	38
TOTAL	53	-	49.25

DEMAND		
ET	49.25	
ADWF	0.27 L/s	
PWF	1.6 L/s	
GI	0.01 L/s	
ROI	0.51 L/s	
DESIGN FLOW	2.29 L/s	
DIPPOWF	1.30	



WATER LOADING CALCULATIONS

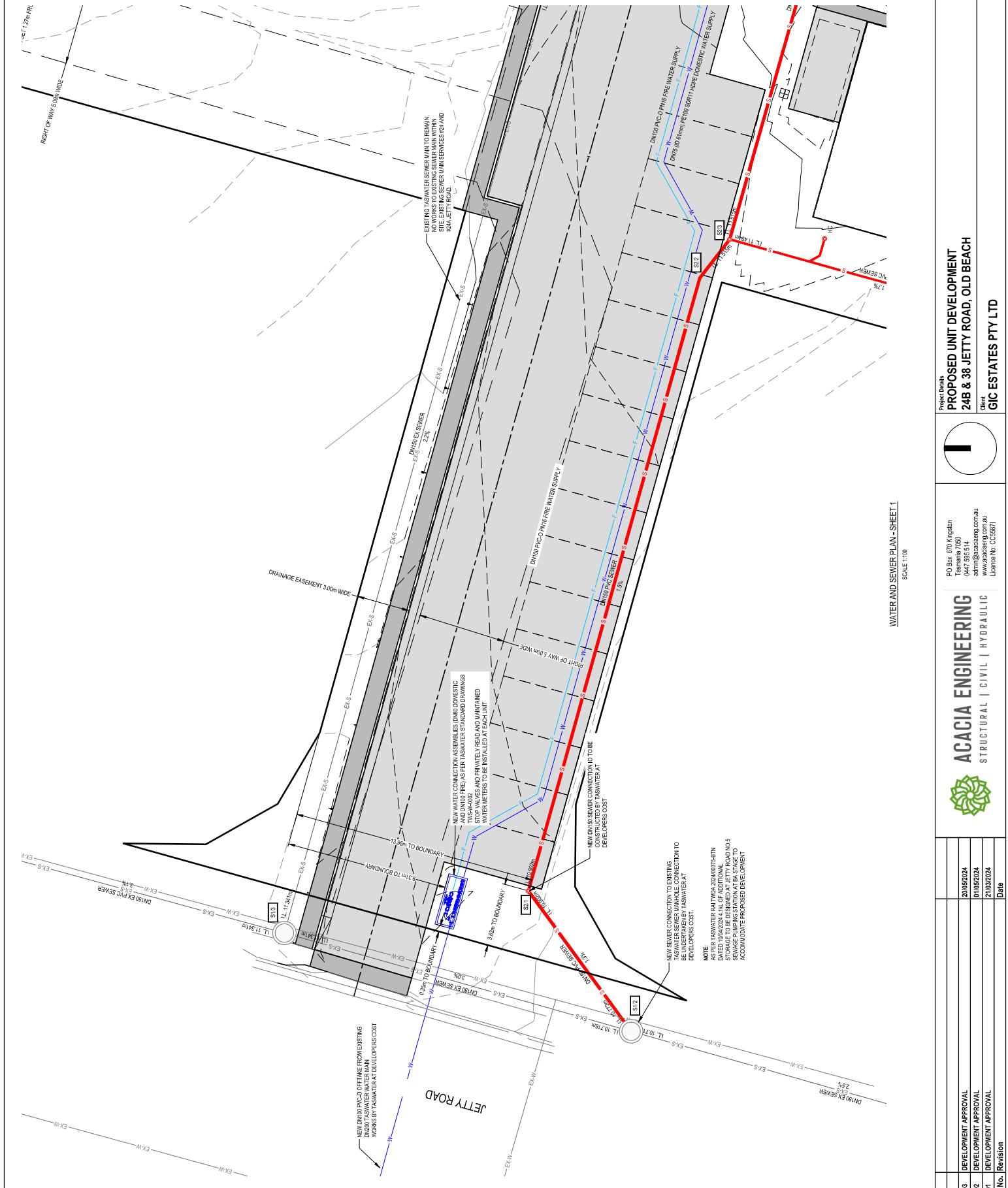
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CODE	NO. OF UNITS	ET PER UNIT	ET
RM02	15	0.6	9
RM03	38	0.8	30.4
TOTAL	53	-	39.4

DEMAND		
ET	39.4	
AD	0.31 L/s	
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PH	1.68 L/s	
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20 PEAK + FIRE	13.27 L/s	

SEWERAGE LOADING CALCULATIONS

LOADINGS			
CODE	NO. OF UNITS	ET PER UNIT	ET
RM02	15	0.75	11.25
RM03	38	1	38
TOTAL	53	-	49.25

DEMAND		
ET	49.25	
ADWF	0.27 L/s	
PWF	1.6 L/s	
GI	0.01 L/s	
ROI	0.51 L/s	
DESIGN FLOW	2.29 L/s	
DIPPOWF	1.30	



Project Name	WATER AND SEWER PLAN - SHEET 1		
Scale	A1	Project No.	2023-02-05
Sheet	1:100	Draw No.	C4001_D3

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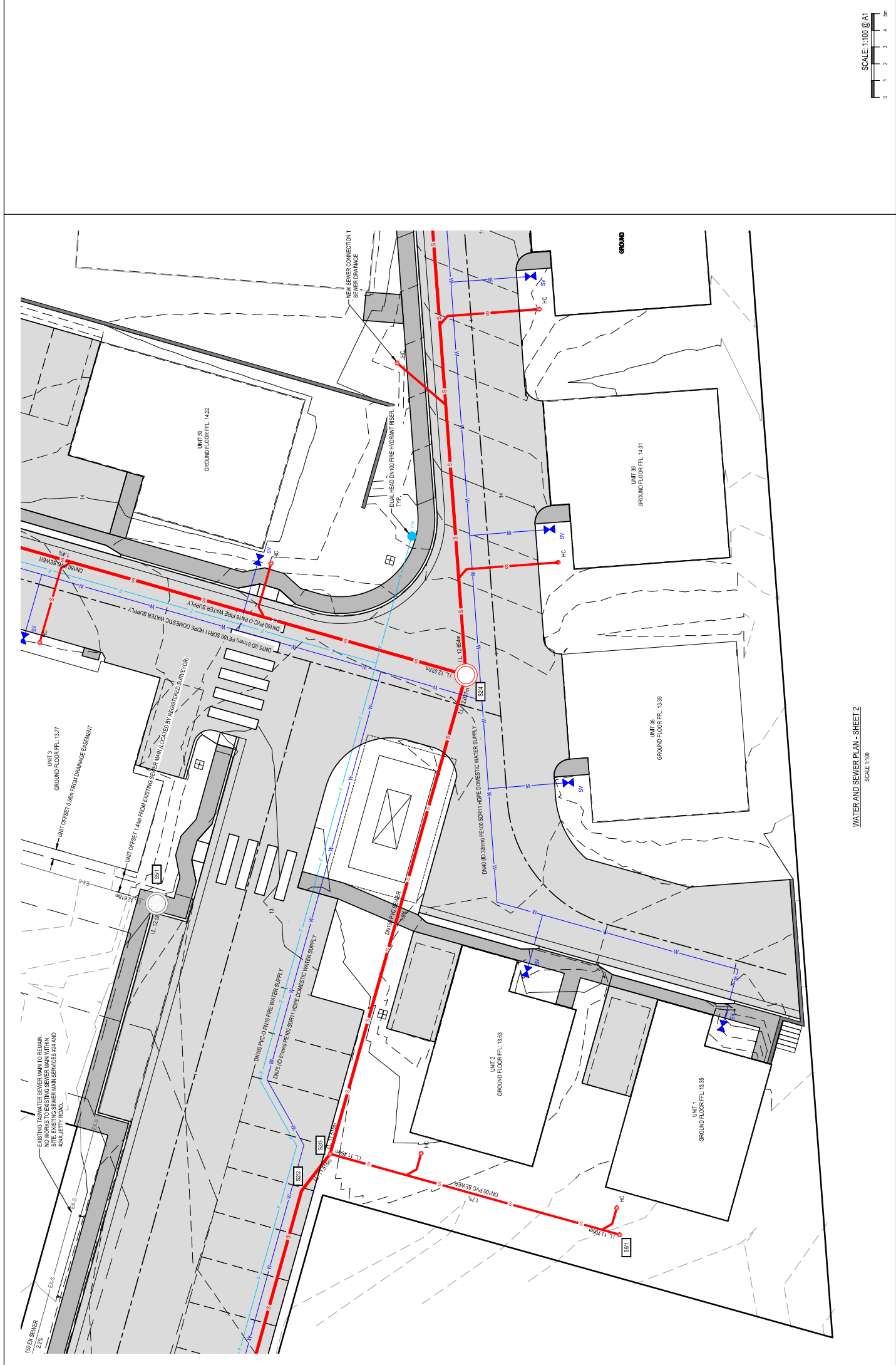
Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Scale
1:100

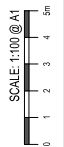
Project No.
2023-02-05


Draw No.
C4001_D3

No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	01/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024



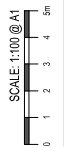
WATER AND SEWER PLAN - SHEET 2
SCALE 1:100




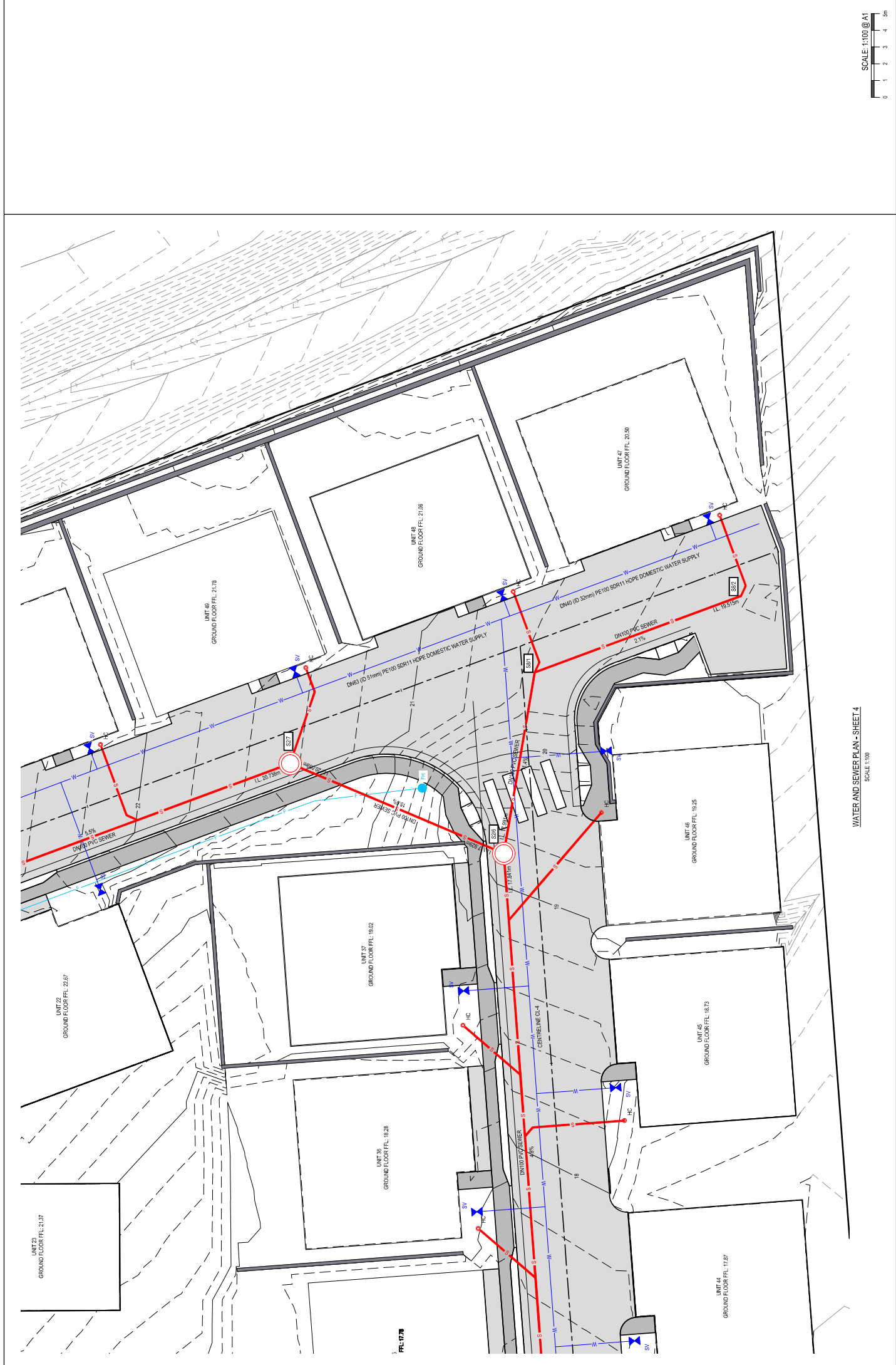
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Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH		Client GIC ESTATES PTY LTD			
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DEVELOPMENT APPROVAL		Date			
DEVELOPMENT APPROVAL					



WATER AND SEWER PLAN - SHEET 3
SCALE 1:100



Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH		Scale 1:100	Sheet No. A1	Project No. 2023-02-05	Draw No. C4003	Rev. D1
Project Client GIC ESTATES PTY LTD		PO Box 670 Woomera Tasmania 7550 0447 595 514 admin@acaciaeng.com.au www.acaciaeng.com.au License No. CC5891				
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Scale	1:100
Sheet	A1
Project No.	2023-02-05
Drawn No.	C4004
Rev.	D1

Drawing Name
WATER AND SEWER PLAN - SHEET 4

Project Name
**PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH**

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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024

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Drawing Name
WATER AND SEWER PLAN - SHEET 4

Project Name
**PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH**

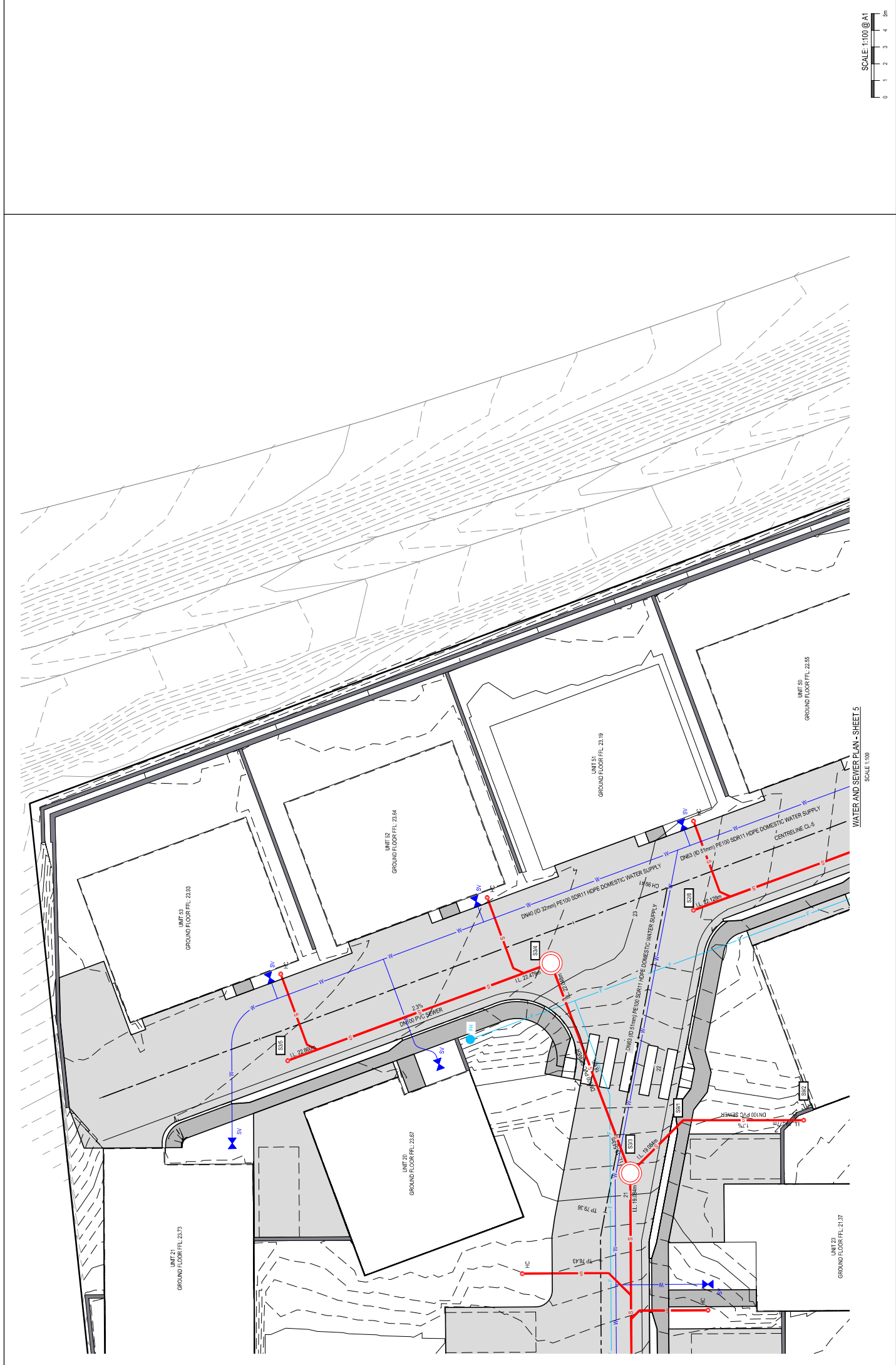
Client
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024

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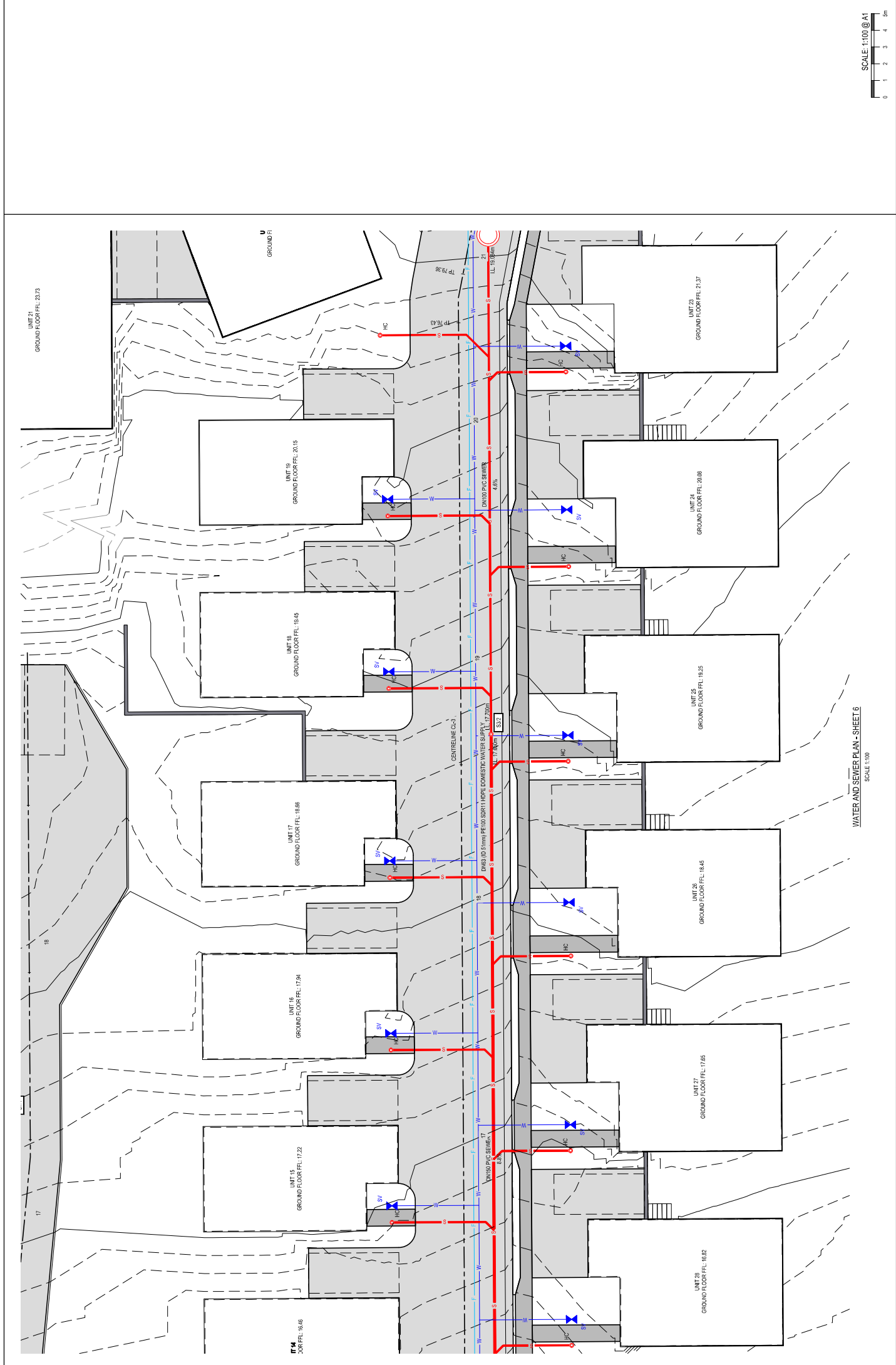


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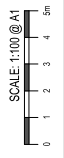
Drawing Name
WATER AND SEWER PLAN - SHEET 5

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	Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH	Client GIC ESTATES PTY LTD

No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D1	DEVELOPMENT APPROVAL	21/03/2024



WATER AND SEWER PLAN - SHEET 6
SCALE 1:100



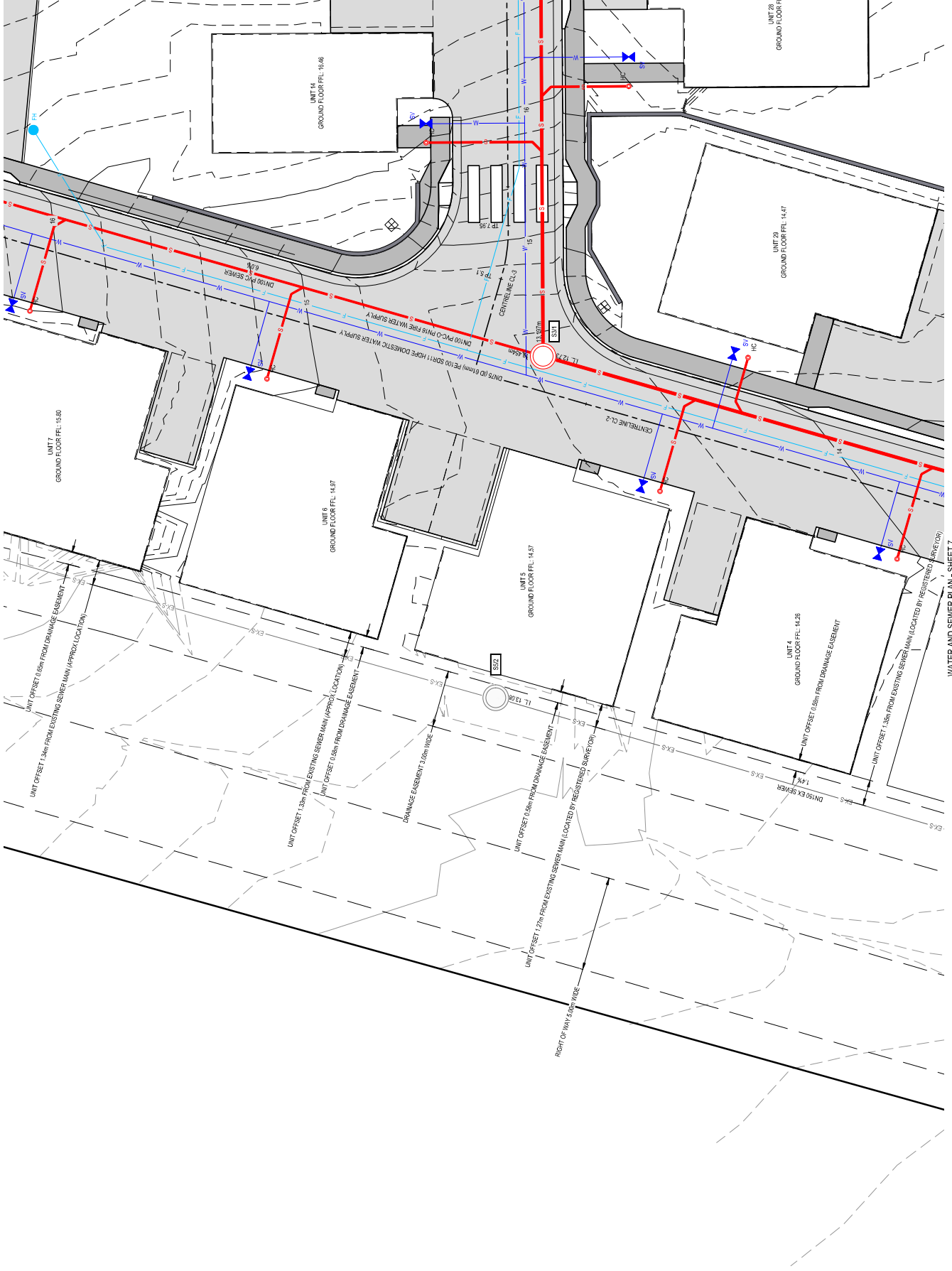
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Scale	A1	Project No.	2023-02-05	Rev.
Sheet	1:100	Rev.	C4006	D1

Project Details
PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH
 Client: GIC ESTATES PTY LTD



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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



WATER AND SEWER PLAN - SHEET 7
SCALE 1:100

SCALE: 1:100 @ A1
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Project Name	WATER AND SEWER PLAN - SHEET 7		
Scale	A1	Draw No.	C4007 D3
Sheet	1:100	Project No.	2023-02-05

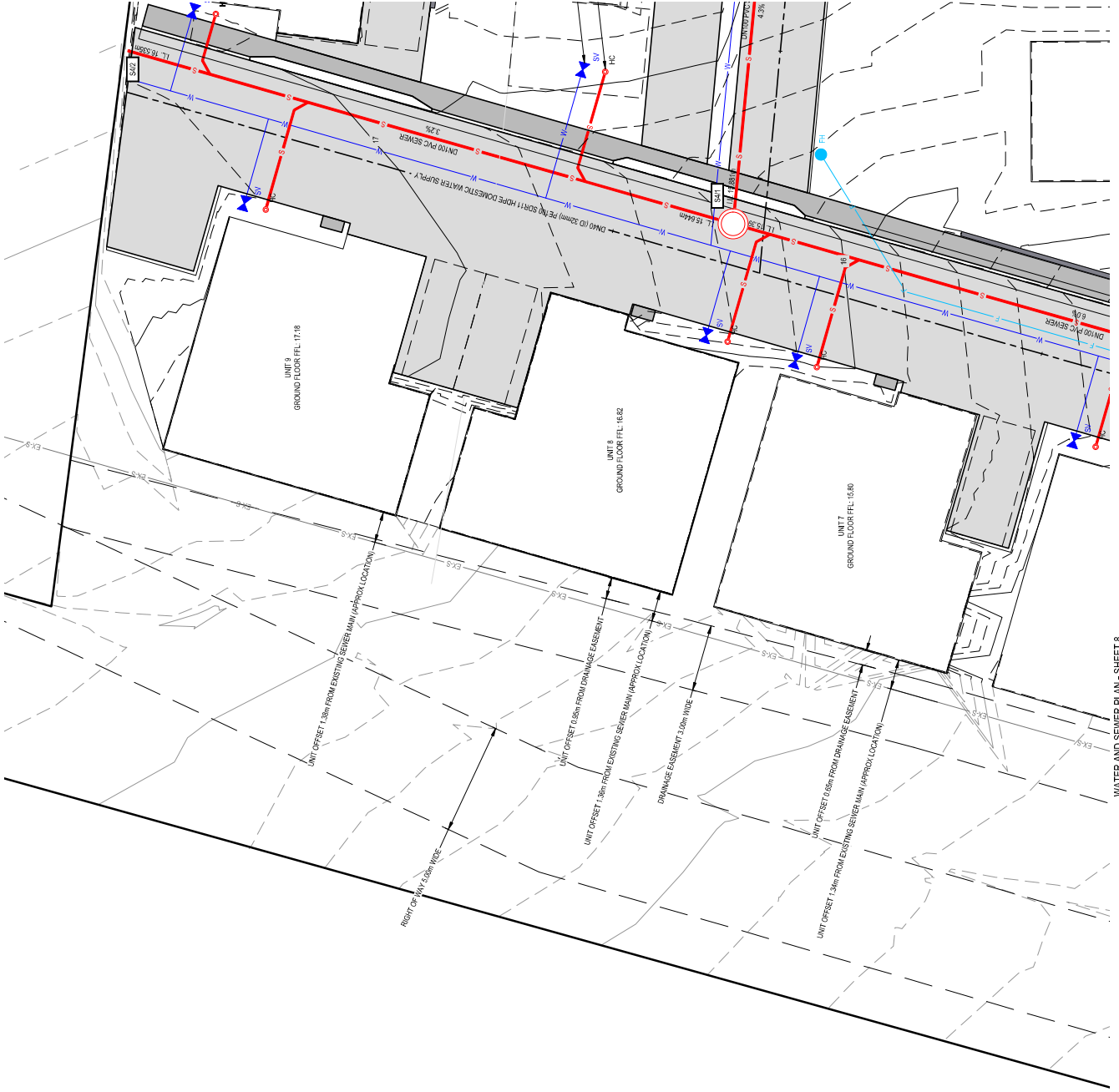
Project Details
PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH
 Client
GIC ESTATES PTY LTD



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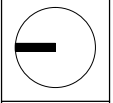
No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D1	DEVELOPMENT APPROVAL	27/03/2024



WATER AND SEWER PLAN - SHEET 8
SCALE 1:100

Scale	1:100
Sheet	A1
Project No.	2023-02-05
Draw No.	C4008
Rev.	D3

Drawing Name
WATER AND SEWER PLAN - SHEET 8



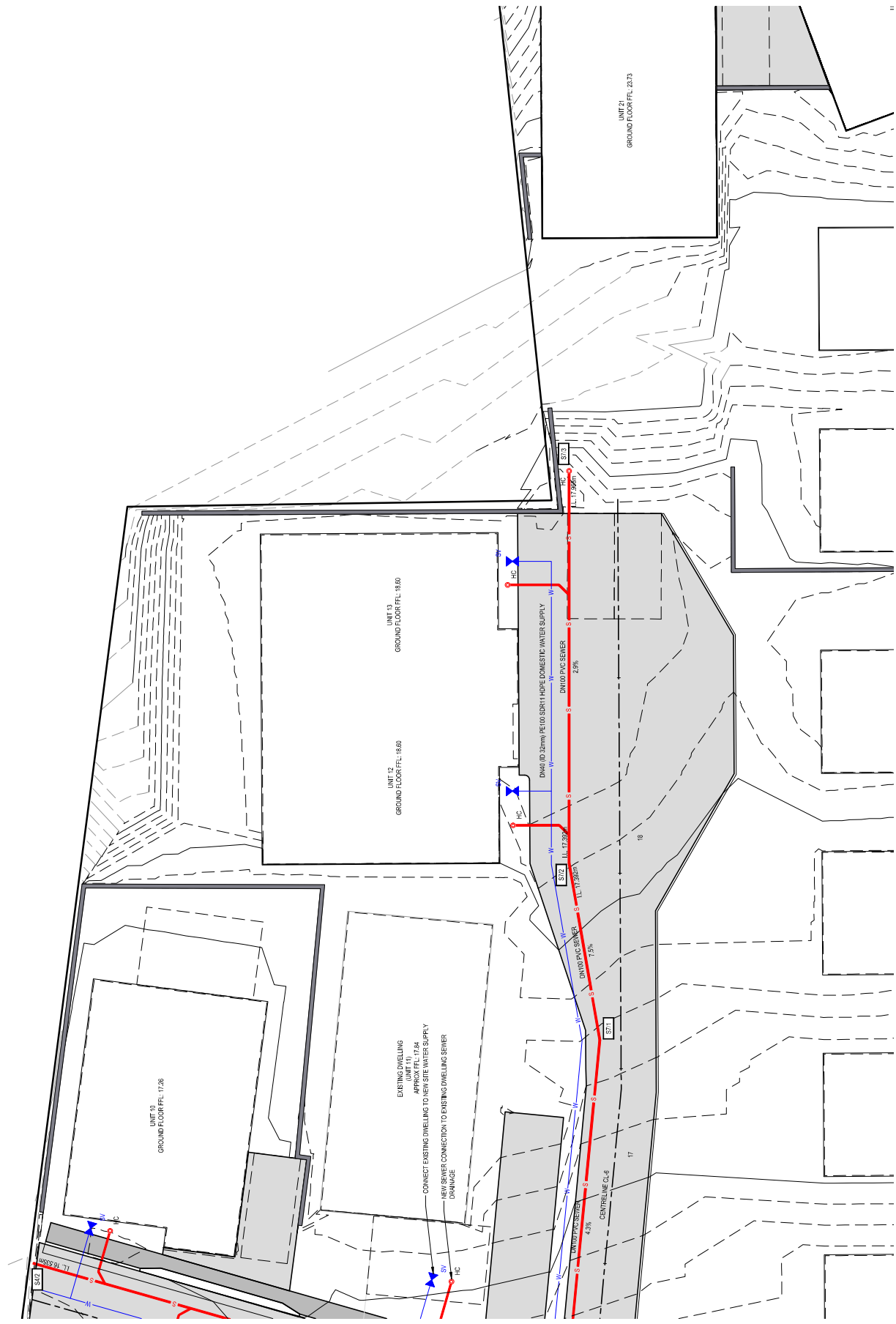
Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Client
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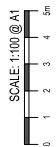
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No.	20152024	Date
D3	DEVELOPMENT APPROVAL	
D1	DEVELOPMENT APPROVAL	21/03/2024
No.	Revision	Date

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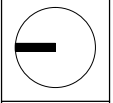
WATER AND SEWER PLAN - SHEET 9
SCALE 1:100



Drawing Name	WATER AND SEWER PLAN - SHEET 9		
Scale	A1	Sheet No.	2023-02-05
Scale	1:100	Draw No.	C4009
		Rev.	D1

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Client
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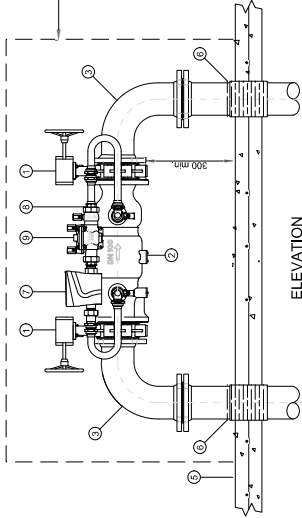


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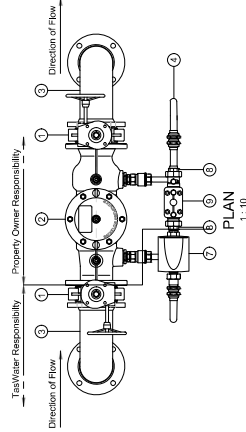
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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024

CAGE AROUND METER ASSEMBLY AS PER TWS-V4003.

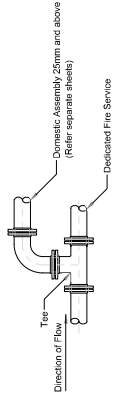


ELEVATION
1:10



PLAN - DOMESTIC ASSEMBLY
1:20

BOUNDARY CONNECTION PARTS LIST		
No.	DESCRIPTION	COMMENTS
1	DN 100 Lug Type Butterfly Valve with Worm Gear Actuator	1. Supplied by TasWater
2	DN 100 Sensus WPF DN 100	Owned, operated and maintained by Property Owner
3	DN 100 Sensus WPF DN 100	Owned, operated and maintained by Property Owner
4	DN 100 PN16 Metallic Water Pipe	Copper not to be used
5	DN 100 PN16 Metallic Water Pipe	Supplied by TasWater
6	100mm (minimum) Reinforced Concrete Slab	SL72 placed central (Refer Note 3)
7	Pipe Wrapped where Concrete will contact Pipe	DN25 Bypass for DN 100 - DN 150
8	Sensus Meter size for Low Flow Bypass	Owned, operated and maintained by property owner
9	DN 20/25 Brass Nipple	
10	Double Check Valve Included With DN 100 Double Detector Check	



BOUNDARY CONNECTION PARTS LIST		
No.	DESCRIPTION	COMMENTS
1	DN 65 Sensus Measurement Plus Water Meter	Supplied by TasWater
2	DN 65 Sensus WPF DN 65	Supplied by TasWater
3	DN 65 Lug Type Butterfly Valve with Worm Gear Actuator	Owned, operated and maintained by Property Owner
4	Low Hazard DN 65 Val-Chief® DCO3U Double Check Valve Non-Resettable	Supplied by TasWater
5	DN 65 PN16 Metallic Water Pipe	Copper not to be used for DN 100
6	DN 65 x 200mm Hydrant Riser with 20mm Tapping for Pressure Testing, 20mm Valve and Plug	SL72 placed central
7	100mm (minimum) Reinforced Concrete Slab	
8	Pipe Wrapped where Concrete will contact Pipe	(Refer Note 5)

VALVE & EQUIPMENT SCHEDULE

- Only use products with watermark certification and approved for use by TasWater and listers with City West Water's approved products catalogue.
- Installation must comply with manufacturer's written instructions, manufacturer's drawings, and applicable standards for valves, fittings, and washers.
- All valves must be solvent sealed, otherwise classed to AS 1628 with 316 stainless steel bolts and washers.
- Unless approved otherwise the water meters, tails and meter boxes are to be supplied by TasWater.

GENERAL NOTES

- All dimensions in millimeters (mm), unless noted otherwise.
- Installation and fittings schedule is also suitable for DN 65 and DN 100 meters.
- Dimensions shown in brackets apply to DN 65 / DN 60 / DN 100.
- A 3mm clearance has been added where a gasket is required.
- All metallic pipe work to be 'Dimeso' wrapped, or equivalent where it comes in contact with concrete - to protect it from corrosion.
- Install and locate the meter assembly so that the meter can be easily read.
- Install the meter assembly in cage in accordance with TWS-V4003.
- The Property Owner is responsible for the ongoing maintenance of the security cage.

VALVE & EQUIPMENT SCHEDULE

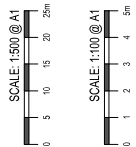
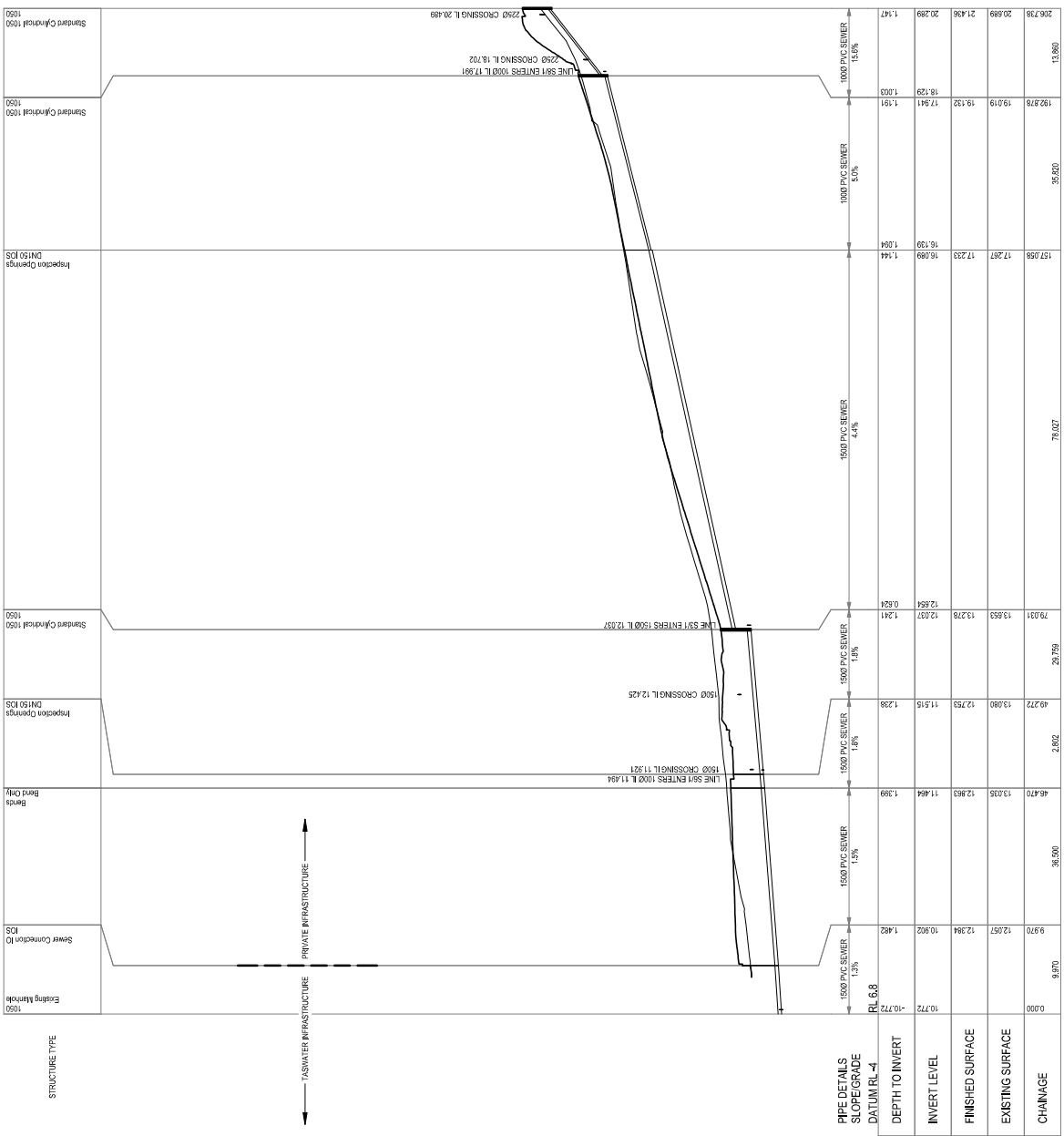
- Only use products with watermark certification and approved for use by TasWater and listed in TasWater's approved products catalogue.
- Installation must comply with manufacturer's written instructions, manufacturer's drawings, and applicable standards for valves, fittings, and washers.
- All valves must be solvent sealed, otherwise classed to AS 1628 with 316 stainless steel bolts and washers.
- Unless approved otherwise the water meters, tails and meter boxes are to be supplied by TasWater.

GENERAL NOTES

- All dimensions in millimeters (mm), unless noted otherwise.
- Install and locate the meter assembly so that the meter can be easily read.
- All metallic pipe work to be 'Dimeso' wrapped, or equivalent where it comes in contact with concrete - to protect it from corrosion.
- Where a vented back-flow prevention device is required such as a Reduced Pressure Zone Device (RPZD), it shall be installed in accordance with AS 4850.1 and AS 2845, and shall:
 - Not be in an area that may be subjected to ponding.
 - Have the relief drain outlet not less than 300mm above the surrounding surface.
- Install the meter assembly in cage in accordance with TWS-V4003.
- The Property Owner is responsible for the ongoing maintenance of the security cage.

No.	Revision	DEVELOPMENT APPROVAL		Date	2/10/2024
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Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH					
Drawing Name WATER METERING DETAILS					
Scale	-	Sheet	A1	Proj. No.	2023-02-05
Rev.	-	Drawn By	C4101	Drawn No.	D1

S102 S101 S100 S204 S203 S202 S201 S206 S205 100



Drawing Name
SEWER LONG SECTIONS - SHEET 1

Scale
 -

Sheet
 A1

Project No.
 2023-02-05

Draw No.
 C4201

Rev.
 D1

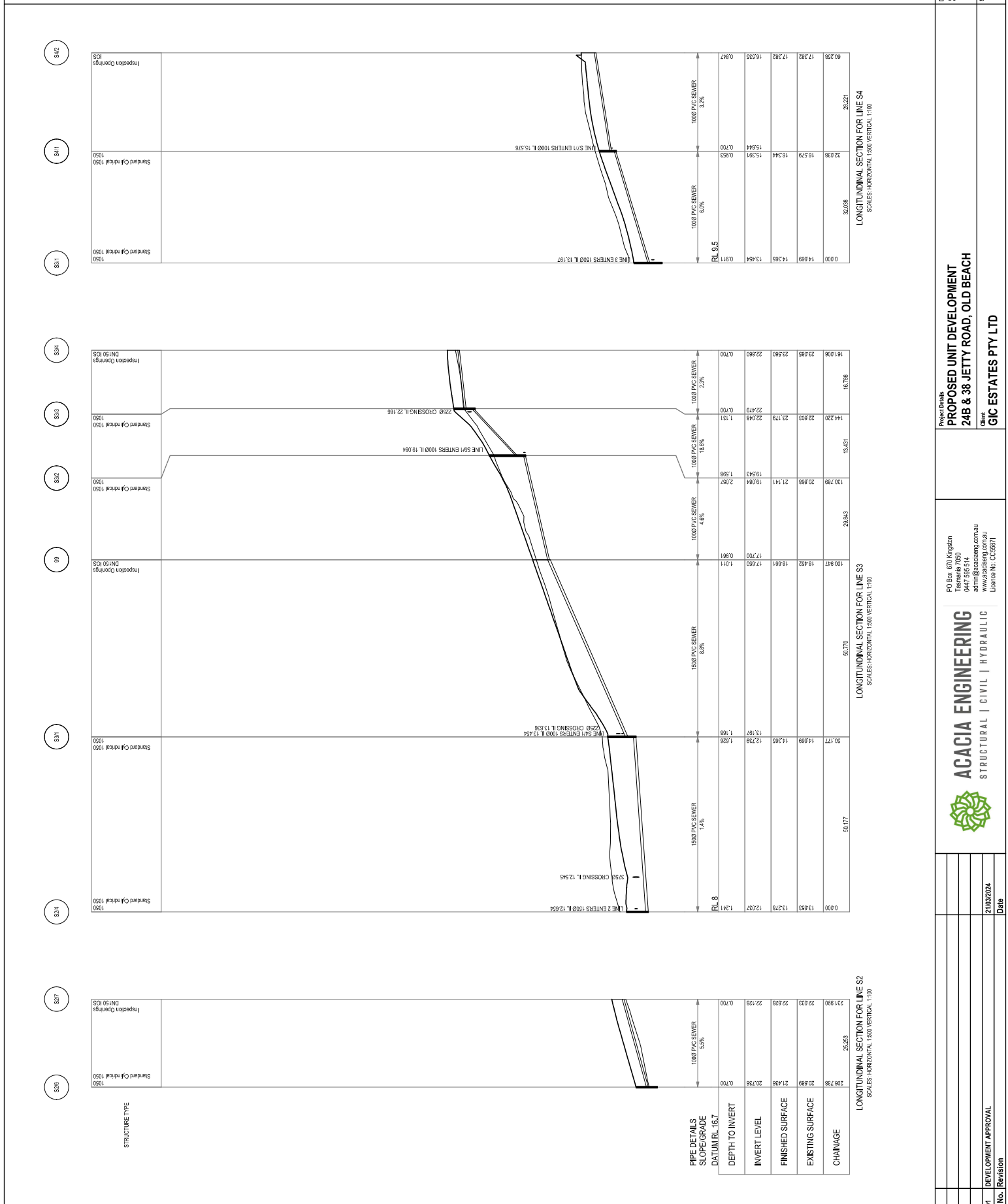
Project Name
**PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH**

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No.	Revision	Date
D1	DEVELOPMENT APPROVAL	21/03/2024



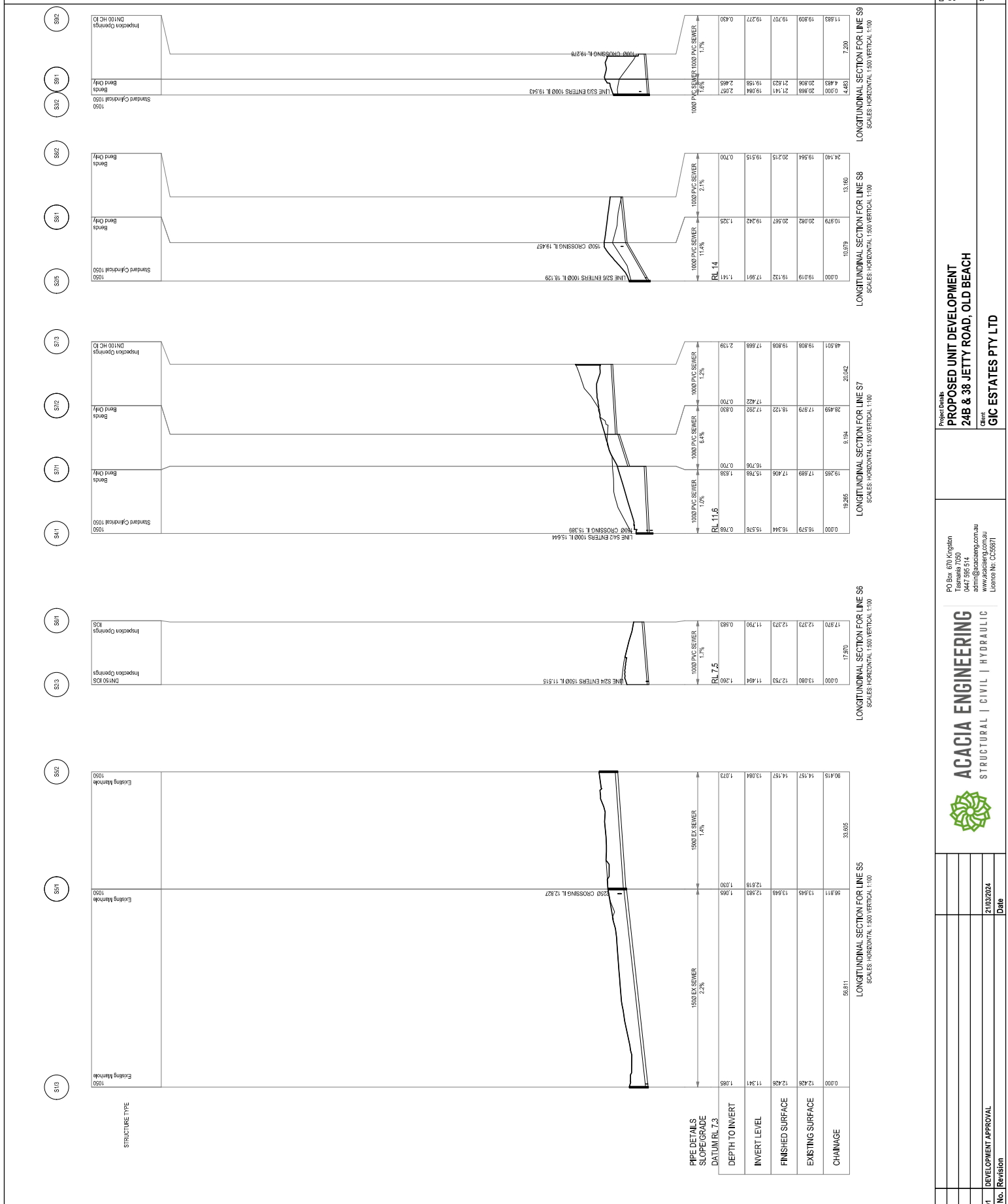
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Project Name: **PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH**

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No.	Revision	Date
1	DEVELOPMENT APPROVAL	21/03/2024



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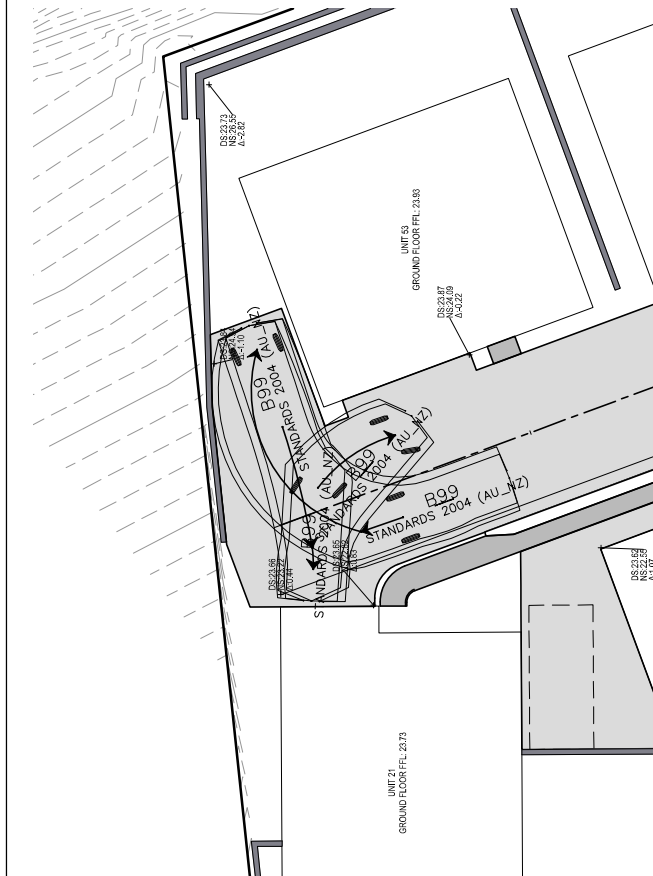
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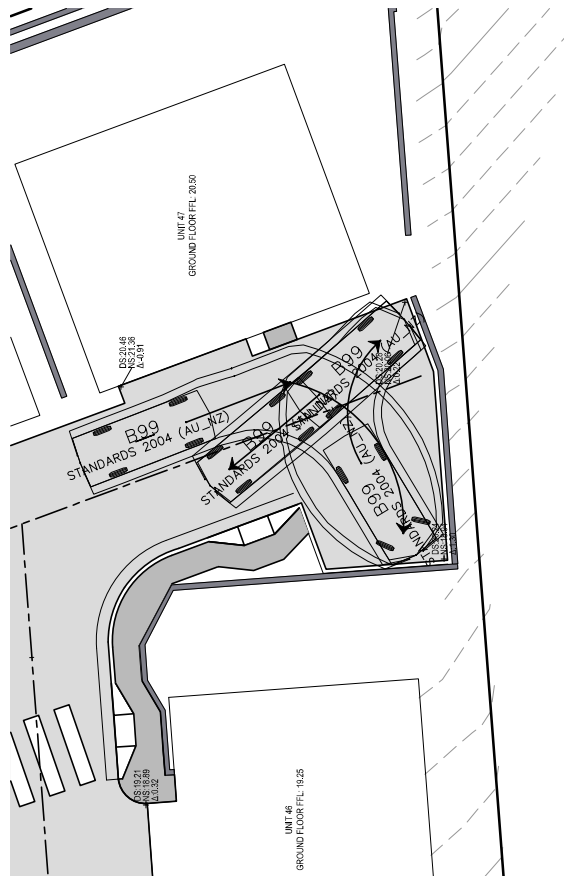
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No.	Revision	Date
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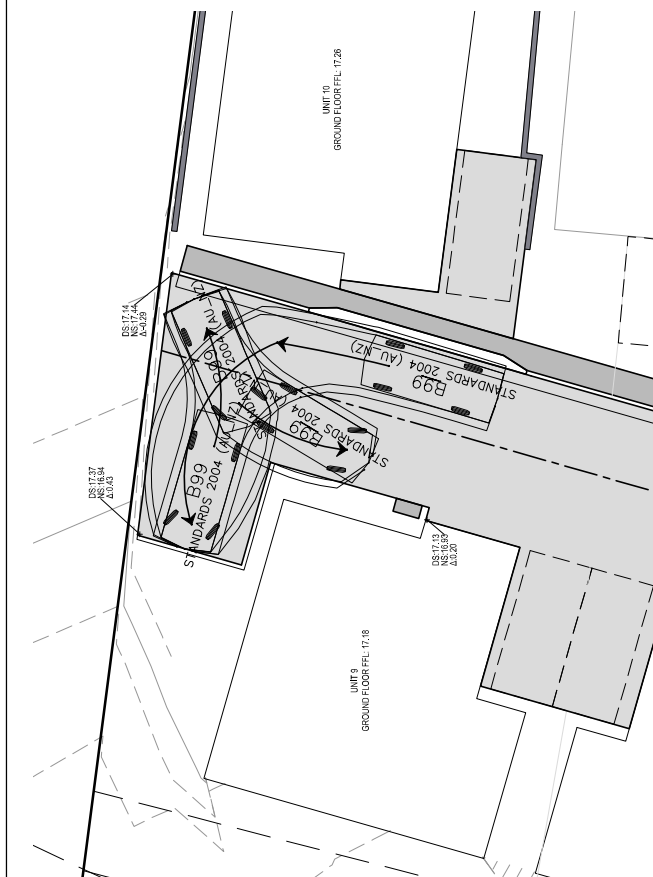
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<p style="text-align: center;">PO Box 670 Wagon Tasmania 7050 0447 595 514 admin@acacieng.com.au www.acacieng.com.au Licence No. CC5891</p>			
			
Project Name PROPOSED UNIT DEVELOPMENT 24B & 38 JETTY ROAD, OLD BEACH			
Client GIC ESTATES PTY LTD			
Drawing Name B99 VEHICLE TURNING PLANS			
Scale	1:100	Sheet No.	A1
Project No.	2023-02-05	Draw No.	C5001
D2			



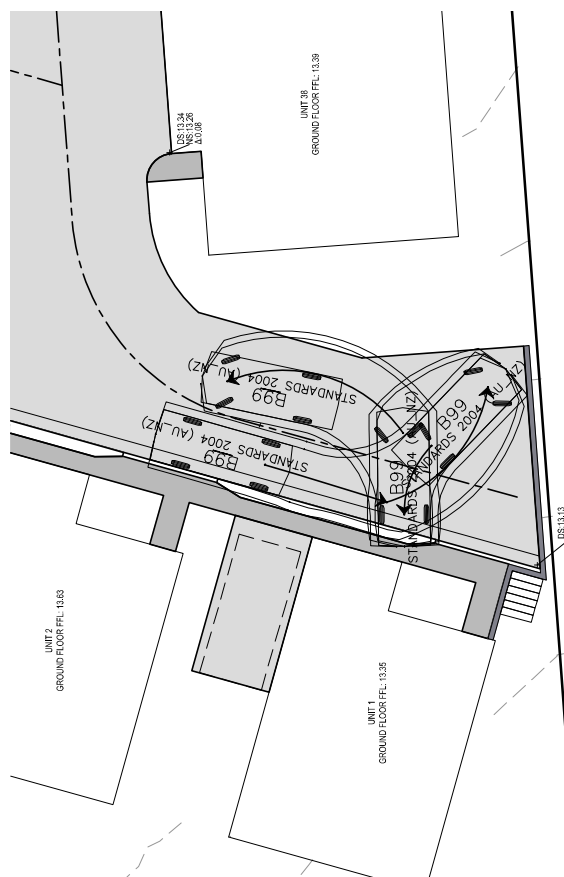
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SCALE 1:100



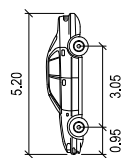
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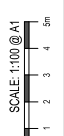
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SCALE 1:100

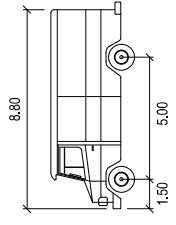
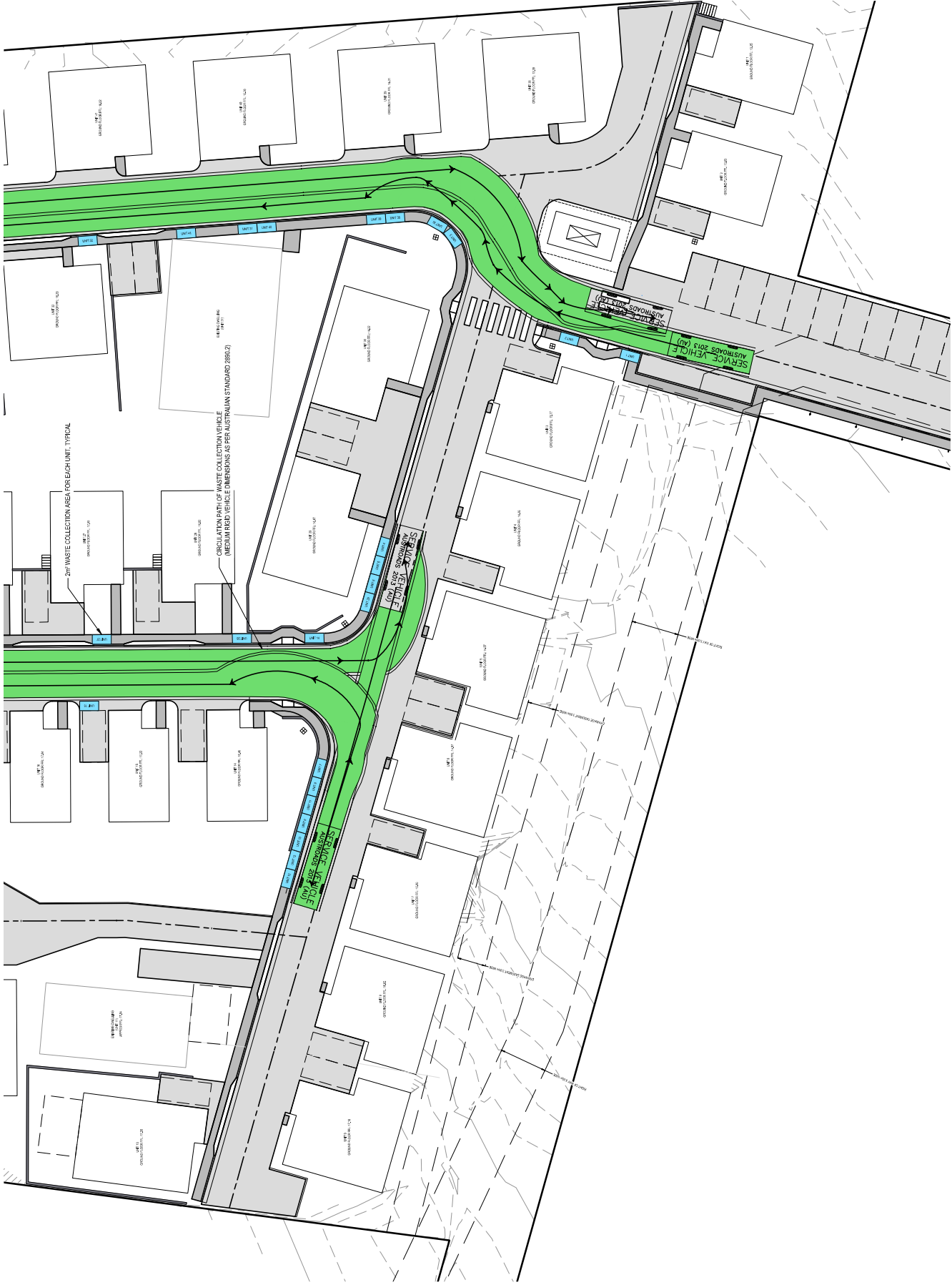


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SCALE 1:100



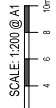
B99
 meters
 Width : 1.94
 Track : 1.84
 Lock to Lock Time : 6.0
 Steering Angle : 33.9





SERVICE VEHICLE meters

- Width : 2.50
- Track : 2.50
- Lock to Lock Time : 6.0
- Steering Angle : 38.7



MEDIUM RIGID VEHICLE TURNING PLAN - SHEET 1
SCALE 1:200

Drawing Name		MEDIUM RIGID VEHICLE TURNING PLAN	
Scale	1:200	Project No.	2023-02-05
Sheet	A1	Draw No.	C5101
		Rev.	D3

Project Name
**PROPOSED UNIT DEVELOPMENT
24B & 38 JETTY ROAD, OLD BEACH**

Client
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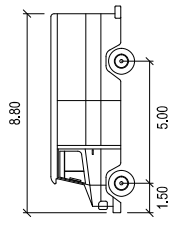
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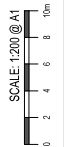
No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	01/05/2024



MEDIUM RIGID VEHICLE TURNING PLAN - SHEET 2
SCALE 1:200



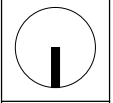
SERVICE VEHICLE meters
 Width : 1.50
 Track : 2.50
 Lock to Lock Time : 6.0
 Steering Angle : 38.7



Drawing Name		MEDIUM RIGID VEHICLE TURNING PLAN	
Scale	1:200	Project No.	2023-02-05
Sheet	A1	Draw No.	C5102
		Rev.	D3

Project Name
PROPOSED UNIT DEVELOPMENT
 24B & 38 JETTY ROAD, OLD BEACH

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No.	Revision	Date
D3	DEVELOPMENT APPROVAL	20/05/2024
D2	DEVELOPMENT APPROVAL	07/05/2024



Planning Assessment
24B & 38 Jetty Road Old
Beach

EAST DERWENT HIGHWAY

10 April 2024

Version Control			
Version	Author	Date	Changes
0	Frazer Read	20.02.2024	Draft
1	Frazer Read	10.4.2024	For DA submission

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1. Introduction

All Urban Planning Pty Ltd has been engaged by GIC Estates as Trustee for Squatter Unit Trust to prepare the following Planning Assessment for Multiple Dwellings at 24B & 38 Jetty Road, Old Beach under the provisions of the Brighton Local Provisions Schedule of the Tasmanian Planning Scheme (planning scheme).

Site & Surrounds

The proposal relates to CT 159864/3 and 159864/1. The site area (less the area of the access strips) is 1.72ha.

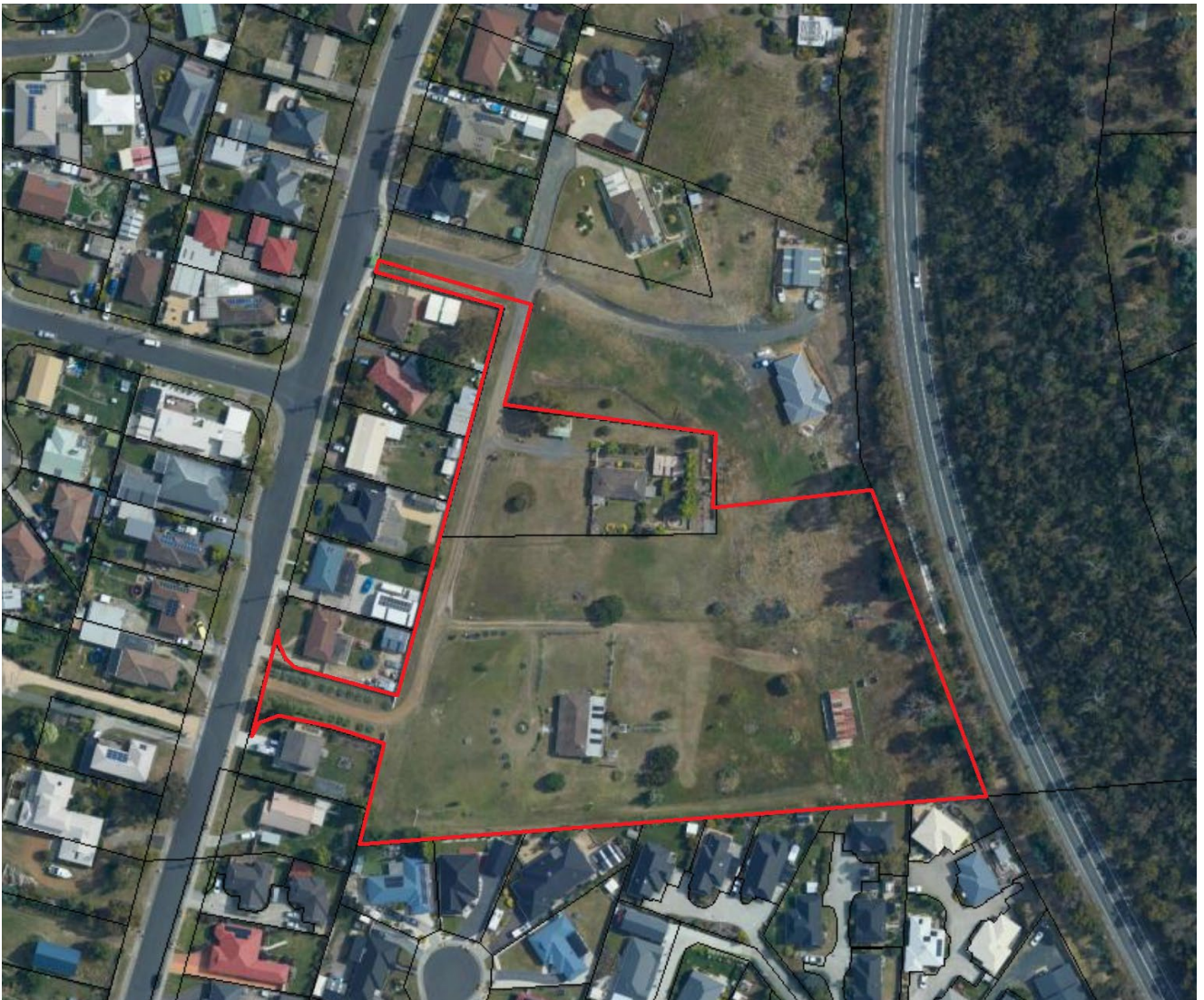


Figure 1– Site Plan (source annotated from theList)



Figure 2 – proposed site plan (Source: Prime Design)

Title Information

The proposal relates to the following land as shown in Figures 1 and 2 above.

Address	Title	Area	Owner
38 Jetty Road Old Beach	159864/3	1.525ha	GIC Estates Pty Ltd
24B Jetty Road Old Beach	159864/1	3121m ²	GIC Estates Pty Ltd

2. Proposal

The proposal is for retention of two existing houses and development of 51 additional dwellings. A total of 53 multiple dwellings consisting of 14 two-bedroom units, 32 three-bedroom units, and seven four-bedroom units.

Units will have 1 of 13 designs; Type: A, B1, B2, C, D, E, F1, F2, F3, F4, G, H1, H2 or I as designated on the site plans.

Existing house on 24B Jetty Road to be renamed Unit 11.

Existing house on 38 Jetty Road to be renamed Unit 32.

The proposal includes 126 carparking spaces including:

- two dedicated parking spaces for each dwelling consisting of either a single enclosed garage and uncovered parking space, double enclosed garage, or two dedicated uncovered parking spaces.
- 19 on-site visitor parking spaces.

The two titles will be adhered in conjunction with the proposal and access will be via the existing 31m frontage to 38 Jetty Road.

A number of communal waste storage areas are proposed for designated units around the site.

3. The Planning Scheme

Under Clause 6.10.1 of the planning scheme the planning authority must, in addition to the matters required by ss51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with ss57(5) of the Act, but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.*

A standard is applicable if the site is within the relevant zone, specific area plan or an area where a site-specific qualification applies and the standard deals with a matter that could affect or be affected by the proposed development; cl.5.6.2.

A standard is defined to mean the objective for a particular planning issue and the means for satisfying that objective through either an acceptable solution or corresponding performance criterion.

Compliance with a standard is achieved by complying with either the acceptable solution or corresponding performance criterion; cl.5.6.3.

The objective of the standard may be considered to help determine whether the proposed use or development complies with the performance criterion of that standard; cl.5.6.4.

Zoning

The land is zoned General Residential.

The Zone Purpose Statements under Clause 8.1 are as follows:

- 8.1.1 *To provide for residential use or development that accommodates a range of dwelling types where full infrastructure services are available or can be provided.*
- 8.1.2 *To provide for the efficient utilisation of available social, transport and other service infrastructure.*
- 8.1.3 *To provide for non-residential use that:*
 - (a) *primarily services the local community; and*
 - (b) *does not cause unreasonable loss of amenity through scale, intensity, noise, activity outside of business hours, traffic generation and movement, or other off site impacts.*
- 8.1.4 *To provide for Visitor Accommodation that is compatible with residential character.*

Local Area Objectives

There are no applicable local area objectives.

Use Table

The proposal involves multiple dwellings within the residential use class and is a permitted use within the zone.

Use Standards

There are no applicable use standards for this permitted residential use.

Development Standards for Dwellings

8.4.1 Residential density for multiple dwellings

Objective:

That the density of multiple dwellings:

- (a) *makes efficient use of land for housing; and*
- (b) *optimises the use of infrastructure and community services.*

Acceptable Solution	Performance Criteria
<p>A1</p> <p><i>Multiple dwellings must have a site area per dwelling of not less than 325m².</i></p>	<p>P1</p> <p><i>Multiple dwellings must only have a site area per dwelling that is less than 325m², if the development</i></p>

	<p><i>will not exceed the capacity of infrastructure services and:</i></p> <p><i>(a) is compatible with the density of existing development on established properties in the area; or</i></p> <p><i>(b) provides for a significant social or community benefit and is:</i></p> <p style="padding-left: 40px;"><i>(i) wholly or partly within 400m walking distance of a public transport stop; or</i></p> <p style="padding-left: 40px;"><i>(ii) wholly or partly within 400m walking distance of an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone or Commercial Zone.</i></p>
<p>Assessment:</p> <p>Complies with A1. The proposal for 53 dwellings on the 17,270m² site equates to a density of 325m² and complies with A1.</p>	

8.4.2 Setbacks and building envelope for all dwellings

Objective:

The siting and scale of dwellings:

- (a) provides reasonably consistent separation between dwellings and their frontage within a street;*
- (b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings;*
- (c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and*
- (d) provides reasonable access to sunlight for existing solar energy installations.*

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>Unless within a building area on a sealed plan, a dwelling, excluding garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is:</i></p> <p><i>(a) if the frontage is a primary frontage, not less than 4.5m, or, if the setback from the primary</i></p>	<p>P1</p> <p><i>A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.</i></p>

<p><i>frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site;</i></p> <p><i>(b) if the frontage is not a primary frontage, not less than 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site;</i></p> <p><i>(c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or</i></p> <p><i>(d) if located above a non-residential use at ground floor level, not less than the setback from the frontage of the ground floor level.</i></p>	
<p>Assessment:</p> <p>There are no buildings within 4.5m of the Jetty road frontage of the site and the proposal complies with A1.</p>	
<p>A2</p> <p><i>A garage or carport for a dwelling must have a setback from a primary frontage of not less than:</i></p> <p><i>(a) 5.5m, or alternatively 1m behind the building line;</i></p> <p><i>(b) the same as the building line, if a portion of the dwelling gross floor area is located above the garage or carport; or</i></p> <p><i>(c) 1m, if the existing ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage.</i></p>	<p>P2</p> <p><i>A garage or carport for a dwelling must have a setback from a primary frontage that is compatible with the setbacks of existing garages or carports in the street, having regard to any topographical constraints.</i></p>
<p>Assessment:</p> <p>There are no garage or carports close to the site frontage and the proposal complies with A2.</p>	
<p>A3</p> <p><i>A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:</i></p>	<p>P3</p> <p><i>The siting and scale of a dwelling must:</i></p> <p><i>(a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:</i></p>

<p><i>(a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by:</i></p> <p><i>(i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and</i></p> <p><i>(ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and</i></p> <p><i>(b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling:</i></p> <p><i>(i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or</i></p> <p><i>(ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser).</i></p>	<p><i>(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;</i></p> <p><i>(ii) overshadowing the private open space of a dwelling on an adjoining property;</i></p> <p><i>(iii) overshadowing of an adjoining vacant property;</i></p> <p>or</p> <p><i>(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;</i></p> <p><i>(b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and</i></p> <p><i>(c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:</i></p> <p><i>(i) an adjoining property; or</i></p> <p><i>(ii) another dwelling on the same site.</i></p>
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Assessment:

The accompanying 3D drapes of the building envelope confirm that the proposal involves some minor exceedances of the permitted building envelope under A3 and must therefore be assessed under P3.

Specifically:

- the ridge line of Units 23 and 24, located in the centre of the site, exceed the 8.5m permitted maximum under A3a)ii); and
- A portion of the eaves of unit 1 protrudes outside the 45 degree envelope under A3a)ii)

The 3D drape also shows that the eaves of Units 21, 40, 41, 42, 43, 45 and 46 each include a minor horizontal protrusion of less than 900mm and comply with A3.

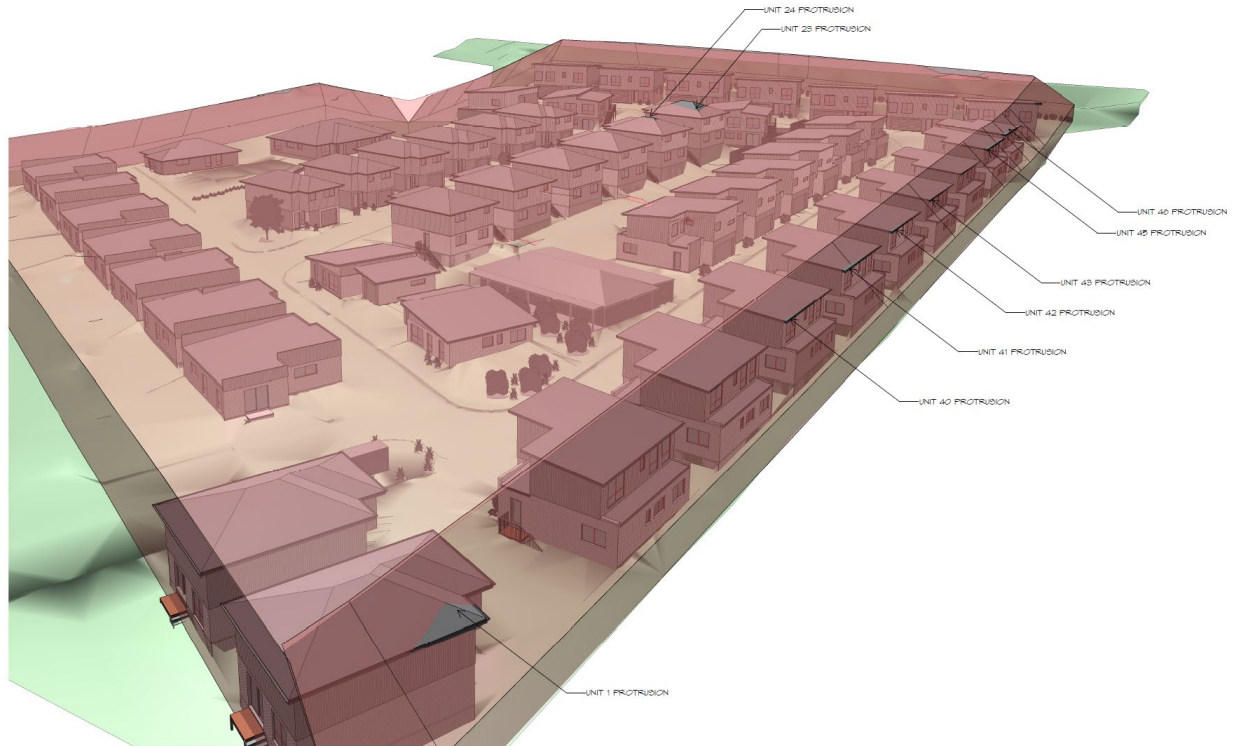
P3 assessment:

Having regard to the above it is only the ridgelines of Units 23 and 24 and a portion of the eaves of Unit 1 that do not comply with the permitted building envelope under A3. All other impacts of the proposal are considered acceptable.

In relation to Units 23 and 24, because they are sited in the middle of the site the additional height will have no shadowing, privacy or visual impact to a dwelling on and adjoining property. No existing solar energy installation either on the site or adjoining will be impacted by the proposal. Therefore, to the extent that the proposal does not satisfy A3 in relation to these dwellings (Units 23 and 24), the proposal is considered to satisfy P3.

The minor variation to the permitted building envelope in relation to the eaves of Unit 1 (see Building Envelope Diagram 1 below) is not considered to have a tangible increased shadowing or visual impact to the adjoining properties to the south at 11 and 13 Coghlan Court (see aerial photo below).

The proposal is considered to satisfy P3.



BUILDING ENVELOPE DIAGRAM 1



Adjacent properties at 11 and 13 Coghlan Court.

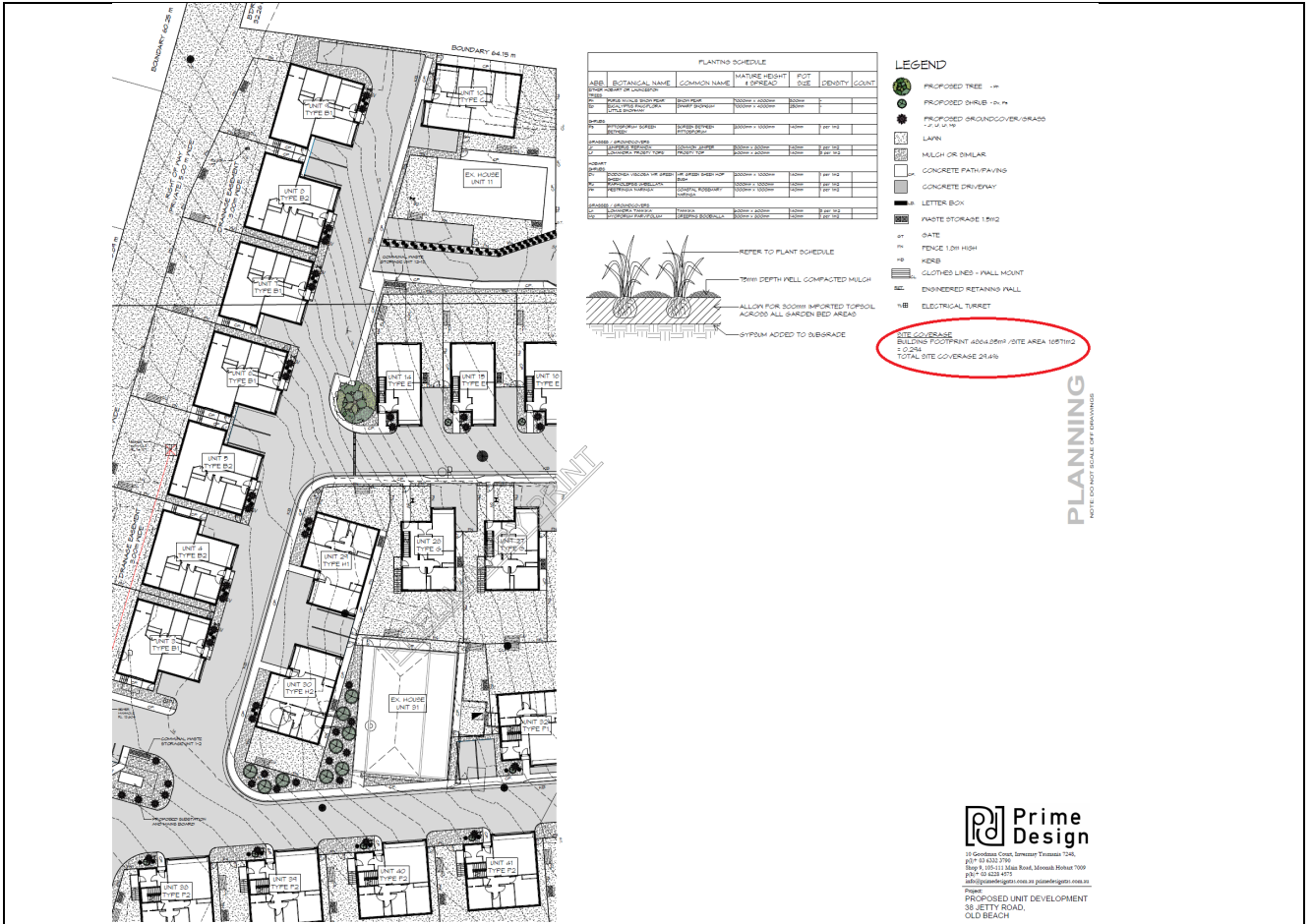
8.4.3 Site coverage and private open space for all dwellings

Objective:

That dwellings are compatible with the amenity and character of the area and provide:

- (a) for outdoor recreation and the operational needs of the residents;*
- (b) opportunities for the planting of gardens and landscaping; and*
- (c) private open space that is conveniently located and has access to sunlight.*

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>Dwellings must have:</i></p> <p><i>(a) a site coverage of not more than 50% (excluding eaves up to 0.6m wide); and</i></p> <p><i>(b) for multiple dwellings, a total area of private open space of not less than 60m² associated with each dwelling, unless the dwelling has a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer).</i></p>	<p>P1</p> <p><i>Dwellings must have:</i></p> <p><i>(a) site coverage consistent with that existing on established properties in the area;</i></p> <p><i>(b) private open space that is of a size and with dimensions that are appropriate for the size of the dwelling and is able to accommodate:</i></p> <ul style="list-style-type: none"> <i>(i) outdoor recreational space consistent with the projected requirements of the occupants and, for multiple dwellings, take into account any common open space provided for this purpose within the development; and</i> <i>(ii) operational needs, such as clothes drying and storage; and</i> <p><i>(c) reasonable space for the planting of gardens and landscaping.</i></p>
<p>Assessment:</p> <p>The proposal complies as follows:</p> <ul style="list-style-type: none"> • The proposal involves a total extent of roofed buildings of 4864m² on the 17270m². This equates to a site coverage of 28% and therefore comfortably complies with A1a). (See drawings 05-06) 	



- The table of 'Lot Areas' on the Site plan sets out the Private Open Space Areas (garden areas) for each dwelling and confirms that all exceed 60m². The proposal therefore complies with A1b).

<p>A2</p> <p>A dwelling must have private open space that:</p> <p>(a) is in one location and is not less than:</p> <ul style="list-style-type: none"> (i) 24m² or (ii) 12m², if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer); <p>(b) has a minimum horizontal dimension of not less than:</p> <ul style="list-style-type: none"> (i) 4m; or (ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 	<p>P2</p> <p>A dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is:</p> <ul style="list-style-type: none"> (a) conveniently located in relation to a living area of the dwelling; and (b) orientated to take advantage of sunlight.
---	---

<p><i>1.8m above the finished ground level (excluding a garage, carport or entry foyer);</i></p> <p><i>(c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and</i></p> <p><i>(d) has a gradient not steeper than 1 in 10.</i></p>	
<p>Assessment:</p> <p>The proposal complies with A2 in that each dwelling has a private open space area of at least 24m² with a minimum dimension of 4m and a gradient not exceeding 1 in 10.</p>	

8.4.4 Sunlight to private open space of multiple dwellings

Objective:

That the separation between multiple dwellings provides reasonable opportunity for sunlight to private open space for dwellings on the same site.

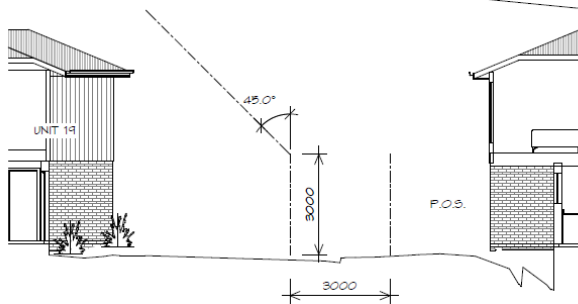
Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>A multiple dwelling, that is to the north of the private open space of another dwelling on the same site, required to satisfy A2 or P2 of clause 8.4.3, must satisfy (a) or (b), unless excluded by (c):</i></p> <p><i>(a) the multiple dwelling is contained within a line projecting (see Figure 8.4):</i></p> <p style="margin-left: 20px;"><i>(i) at a distance of 3m from the northern edge of the private open space; and</i></p> <p style="margin-left: 20px;"><i>(ii) vertically to a height of 3m above existing ground level and then at an angle of 45 degrees from the horizontal;</i></p> <p><i>(b) the multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight between 9.00am and 3.00pm on 21st June; and</i></p> <p><i>(c) this Acceptable Solution excludes that part of a multiple dwelling consisting of:</i></p> <p style="margin-left: 20px;"><i>(i) an outbuilding with a building height not more than 2.4m; or</i></p>	<p>P1</p> <p><i>A multiple dwelling must be designed and sited to not cause an unreasonable loss of amenity by overshadowing the private open space, of another dwelling on the same site, which is required to satisfy A2 or P2 of clause 8.4.3 of this planning scheme.</i></p>

(ii) protrusions that extend not more than 0.9m horizontally from the multiple dwelling.

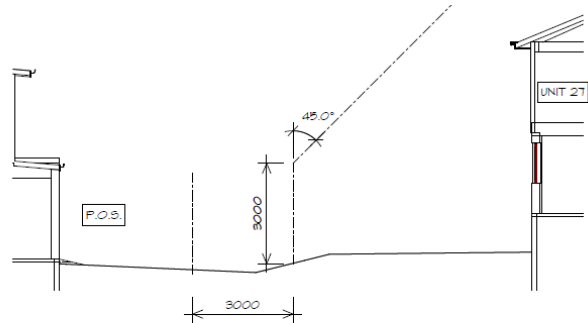
Assessment:

The proposal complies.

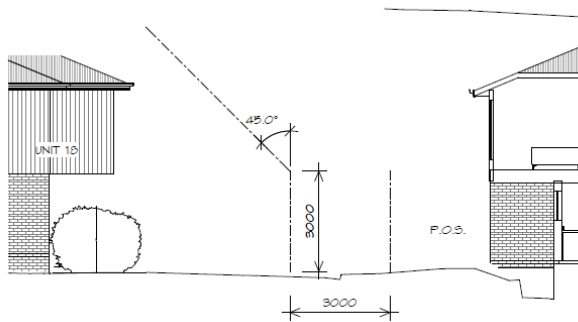
The accompanying sections on drawing 08 confirm (see examples below) that the POS areas are separated in accordance with the envelope setback (Figure 8.4) under A1a.



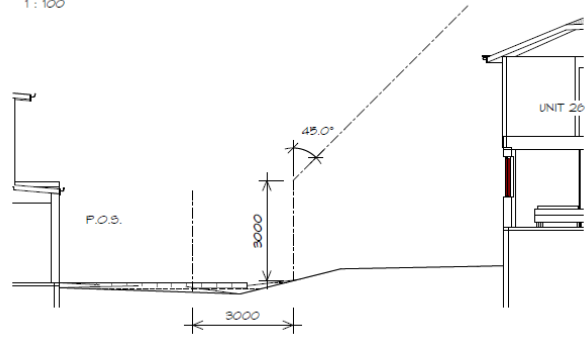
UNIT 19 POS SECTION
1: 100



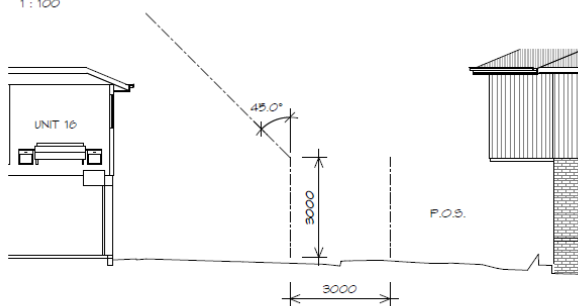
UNIT 27 POS SECTION
1: 100



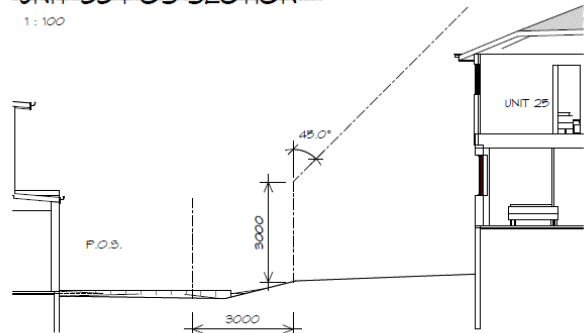
UNIT 10 POS SECTION
1: 100



UNIT 26 POS SECTION
1: 100



UNIT 16 POS SECTION
1: 100



UNIT 25 POS SECTION
1: 100

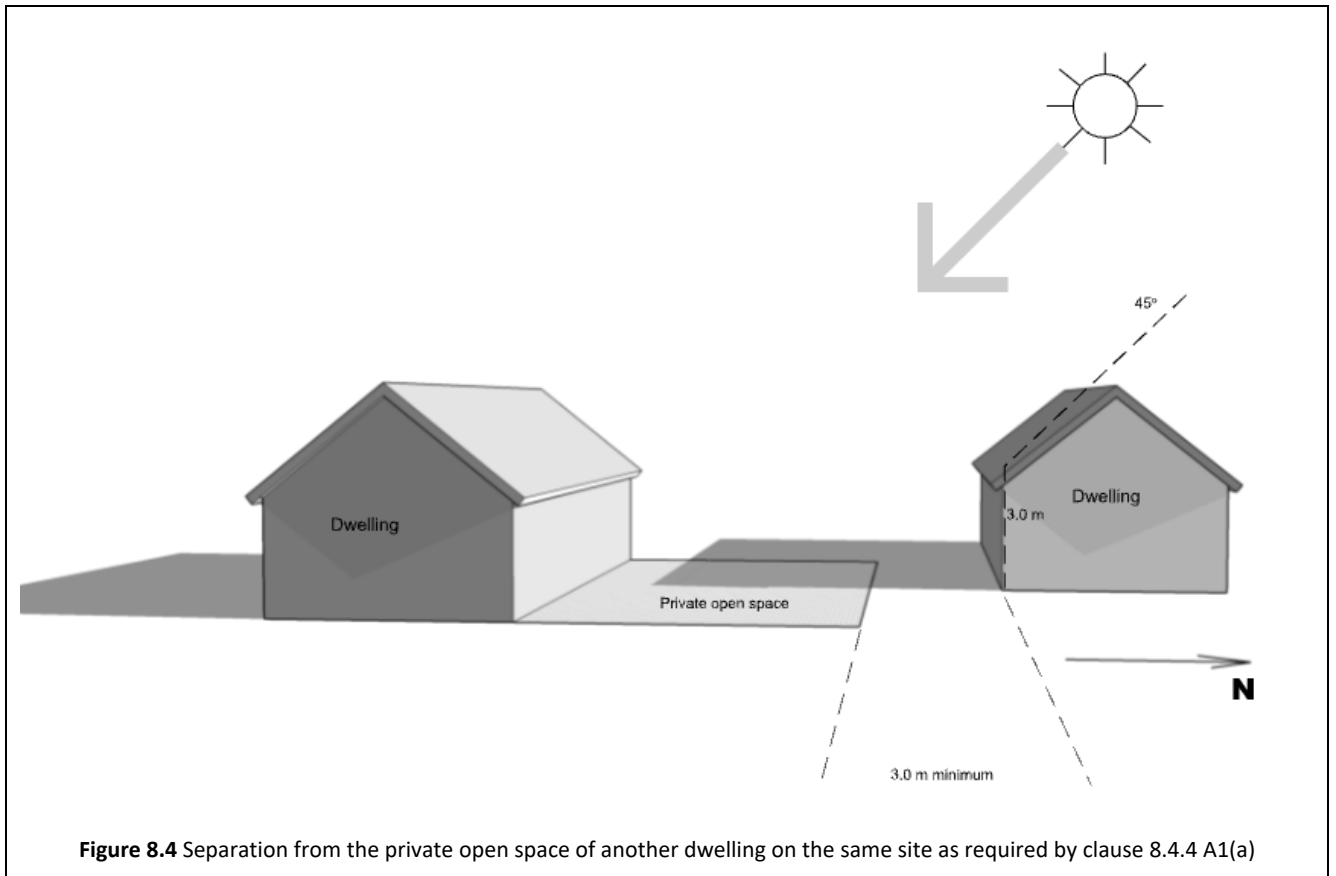


Figure 8.4 Separation from the private open space of another dwelling on the same site as required by clause 8.4.4 A1(a)

8.4.5 Width of openings for garages and carports for all dwellings

Objective:

To reduce the potential for garage or carport openings to dominate the primary frontage.

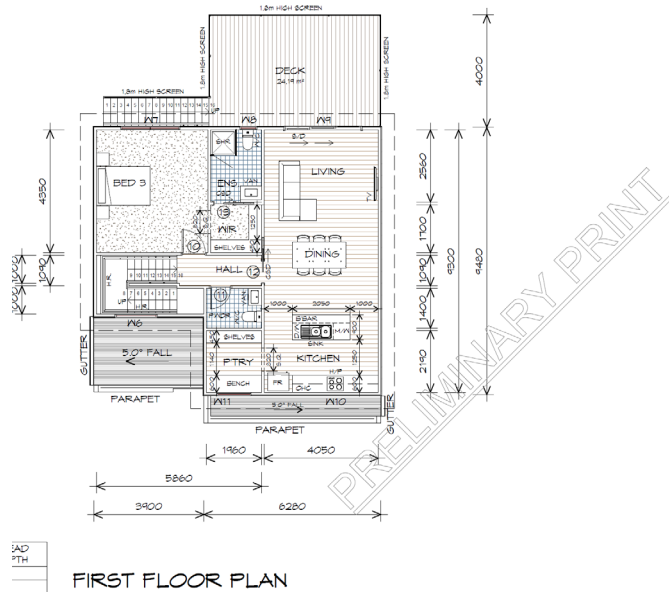
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>A garage or carport for a dwelling within 12m of a primary frontage, whether the garage or carport is free-standing or part of the dwelling, must have a total width of openings facing the primary frontage of not more than 6m or half the width of the frontage (whichever is the lesser).</p>	<p>P1</p> <p>A garage or carport for a dwelling must be designed to minimise the width of its openings that are visible from the street, so as to reduce the potential for the openings of a garage or carport to dominate the primary frontage.</p>
<p>Assessment:</p> <p>There are no garages within 12m of the frontage. The proposal complies.</p>	

8.4.6 Privacy for all dwellings

Objective:

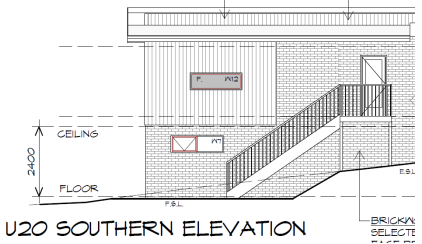
To provide a reasonable opportunity for privacy for dwellings.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p>A balcony, deck, roof terrace, parking space, or carport for a dwelling (whether freestanding or part of the dwelling), that has a finished surface or floor level more than 1m above existing ground level must have a permanently fixed screen to a height of not less than 1.7m above the finished surface or floor level, with a uniform transparency of not more than 25%, along the sides facing a:</p> <p>(a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 3m from the side boundary;</p> <p>(b) rear boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 4m from the rear boundary; and</p> <p>(c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is not less than 6m:</p> <p>(i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or</p> <p>(ii) from a balcony, deck, roof terrace or the private open space of the other dwelling on the same site.</p>	<p>P1</p> <p>A balcony, deck, roof terrace, parking space or carport for a dwelling (whether freestanding or part of the dwelling) that has a finished surface or floor level more than 1m above existing ground level, must be screened, or otherwise designed, to minimise overlooking of:</p> <p>(a) a dwelling on an adjoining property or its private open space; or</p> <p>(b) another dwelling on the same site or its private open space.</p>
<p>Assessment:</p> <p>A number of screen shots of the relevant plans and elevations are included below that demonstrate that the proposal complies with A1.</p> <p>The proposal includes the following decks/balconies:</p> <ul style="list-style-type: none"> • First floor decks on Units 35, 36 and 37 	



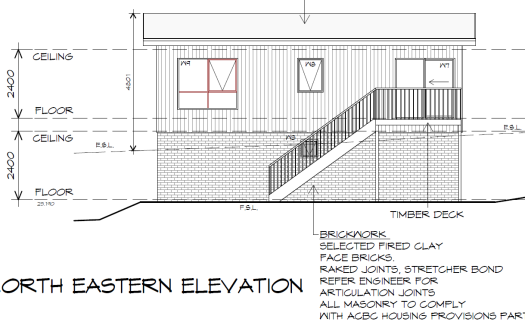
FIRST FLOOR PLAN

- stairs/landings associated with Units 20, 23, 24, 25, 26, 27 and 28



U20 SOUTHERN ELEVATION

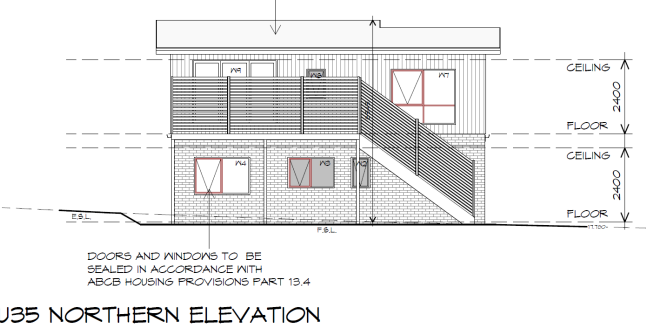
- stairs and landings on Units 47-53.



U51 NORTH EASTERN ELEVATION
1:100

BRICKWORK
SELECTED FIRED CLAY
FACE BRICKS,
RAKED JOINTS, STRETCHER BOND
REFER ENGINEER FOR
ARTICULATION JOINTS
ALL MASONRY TO COMPLY
WITH ASGB HOUSING PROVISIONS PART

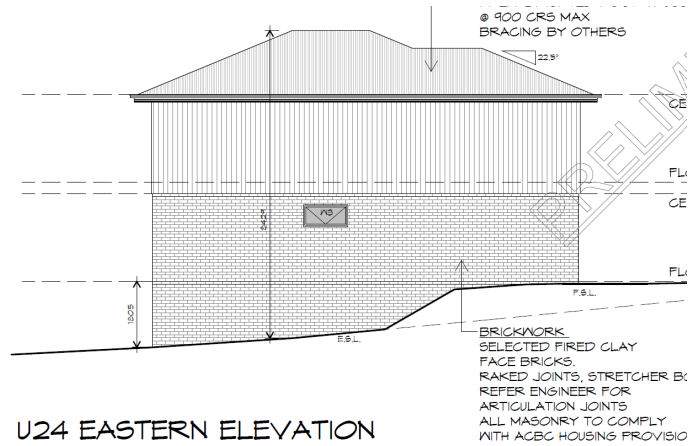
The decks on Units 35, 36 and 37 include fixed screens that comply with A1.



U35 NORTHERN ELEVATION

DOORS AND WINDOWS TO BE
SEALED IN ACCORDANCE WITH
ASGB HOUSING PROVISIONS PART 13.4

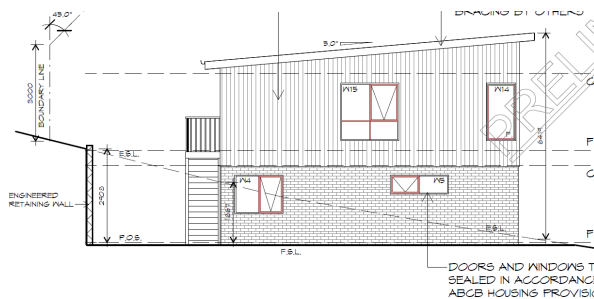
The landings Units 20, 23, 24, 25, 26, 27 and 28 are arranged so that they don't correspond with windows on adjacent dwellings and therefore comply with A1c). Specifically, the eastern elevations of the corresponding dwellings do not include windows



U24 EASTERN ELEVATION

1: 100

The stairs/landings on Units 47-53 are sited low to the existing ground level such that their FFL is less than 1m above existing ground level. A1/P1 therefore does not apply.



U51 NORTH WESTERN ELEVATION

A2

A window or glazed door to a habitable room of a dwelling, that has a floor level more than 1m above

P2

A window or glazed door to a habitable room of a dwelling that has a floor level more than 1m above

<p>existing ground level, must satisfy (a), unless it satisfies (b):</p> <p>(a) the window or glazed door:</p> <p>(i) is to have a setback of not less than 3m from a side boundary;</p> <p>(ii) is to have a setback of not less than 4m from a rear boundary;</p> <p>(iii) if the dwelling is a multiple dwelling, is to be not less than 6m from a window or glazed door, to a habitable room, of another dwelling on the same site; and</p> <p>(iv) if the dwelling is a multiple dwelling, is to be not less than 6m from the private open space of another dwelling on the same site.</p> <p>(b) the window or glazed door:</p> <p>(i) is to be offset, in the horizontal plane, not less than 1.5m from the edge of a window or glazed door, to a habitable room of another dwelling;</p> <p>(ii) is to have a sill height of not less than 1.7m above the floor level or have fixed obscure glazing extending to a height of not less than 1.7m above the floor level; or</p> <p>(iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of not less than 1.7m above floor level, with a uniform transparency of not more than 25%.</p>	<p>existing ground level, must be screened, or otherwise located or designed, to minimise direct views to:</p> <p>(a) a window or glazed door, to a habitable room of another dwelling; and</p> <p>(b) the private open space of another dwelling.</p>
<p>Assessment:</p> <p>The proposal complies with A2 in that all windows that relate to a FFL more than 1m above existing ground level are either:</p> <ul style="list-style-type: none"> • sited more than 3m from a side boundary or 4m from a rear boundary • are located so that they do not correspond with the window of another window of other dwellings on the site within 6m; • or have a sill height greater than 1.7m above FFL or an applied screen/fixed obscure glazing. 	
<p>A3</p> <p>A shared driveway or parking space (excluding a parking space allocated to that dwelling) must be separated from a window, or glazed door, to a</p>	<p>P3</p> <p>A shared driveway or parking space (excluding a parking space allocated to that dwelling), must be screened, or otherwise located or designed, to minimise unreasonable impact of vehicle noise or</p>

<p><i>habitable room of a multiple dwelling by a horizontal distance of not less than:</i></p> <p>(a) 2.5m; or</p> <p>(b) 1m if:</p> <p style="padding-left: 20px;"><i>(i) it is separated by a screen of not less than 1.7m in height; or</i></p> <p style="padding-left: 20px;"><i>(ii) the window, or glazed door, to a habitable room has a sill height of not less than 1.7m above the shared driveway or parking space, or has fixed obscure glazing extending to a height of not less than 1.7m above the floor level.</i></p>	<p><i>vehicle light intrusion to a habitable room of a multiple dwelling.</i></p>
<p>Assessment:</p> <p>To the extent that the proposal includes any ground floor windows within 2.5m of the shared driveway, these windows will be setback at least 1m from the edge of the driveway and will have fixed obscure glazing to comply with A3b). It is recommended that this be conditioned on the permit.</p>	

8.4.7 Frontage fences for all dwellings

Objective:

The height and transparency of frontage fences:

- (a) provides adequate privacy and security for residents;*
- (b) allows the potential for mutual passive surveillance between the road and the dwelling; and*
- (c) is reasonably consistent with that on adjoining properties.*

Acceptable Solutions	Performance Criteria
<p><i>No Acceptable Solution. [S5]</i></p>	<p><i>A fence (including a free-standing wall) for a dwelling within 4.5m of a frontage must:</i></p> <p><i>(a) provide for security and privacy while allowing for passive surveillance of the road; and</i></p> <p><i>(b) be compatible with the height and transparency of fences in the street, having regard to:</i></p> <ul style="list-style-type: none"> <i>(i) the topography of the site; and</i> <i>(ii) traffic volumes on the adjoining road.</i>
<p>Footnotes</p> <p><i>[S5] An exemption applies for fences in this zone – see Table 4.6.</i></p>	

Assessment:

To the extent that the proposal may include a front fence it will comply with the exemption under Table 4.6.

8.4.8 Waste storage for multiple dwellings

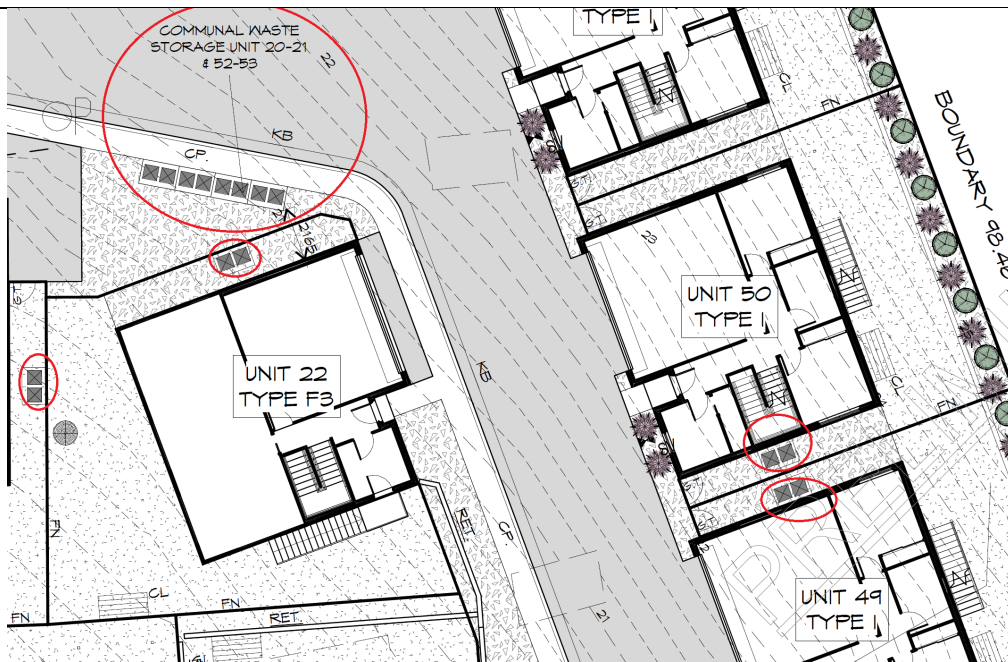
Objective:

To provide for the storage of waste and recycling bins for multiple dwellings.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>A multiple dwelling must have a storage area, for waste and recycling bins, that is not less than 1.5m² per dwelling and is within one of the following locations:</i></p> <p><i>(a) an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or</i></p> <p><i>(b) a common storage area with an impervious surface that:</i></p> <p><i>(i) has a setback of not less than 4.5m from a frontage;</i></p> <p><i>(ii) is not less than 5.5m from any dwelling; and</i></p> <p><i>(iii) is screened from the frontage and any dwelling by a wall to a height not less than 1.2m above the finished surface level of the storage area.</i></p>	<p>P1</p> <p><i>A multiple dwelling must have storage for waste and recycling bins that is:</i></p> <p><i>(a) capable of storing the number of bins required for the site;</i></p> <p><i>(b) screened from the frontage and dwellings; and</i></p> <p><i>(c) if the storage area is a common storage area, separated from dwellings on the site to minimise impacts caused by odours and noise.</i></p>

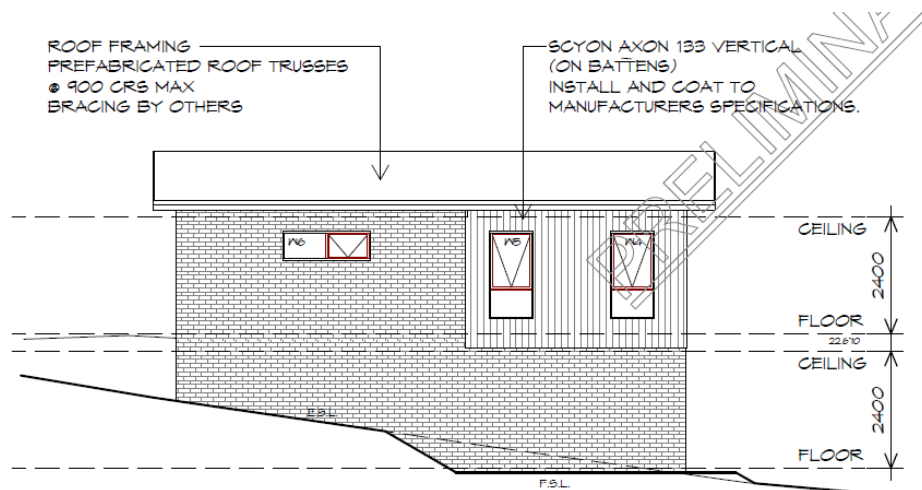
Assessment:

The proposed dwellings either include an area of at least 1.5m² for exclusive bin storage use or access to a designated communal wheelie bin storage area (see Drawings 04 and 05).



Example of the proposal for each dwelling to either have a designated bin storage area for exclusive use by that dwelling or access to a designated communal bin storage area.

The communal bin storage area for Units 20-21 and 52-53 is located within 5m of the side wall of Unit 22 and requires assessment under P1. In this case this storage area is considered to satisfy P1 in that it is located away from the Jetty Road frontage, has sufficient area for 2 bins for each dwelling (8) and is located and sufficiently separated to avoid odour and noise disturbance to nearby dwellings. This is particularly so given that the communal storage area is adjacent/below the garage area of Unit 22 and provides for only a small number dwellings (4 dwellings).



U22 NORTHERN ELEVATION

1:100

Diagram showing the northern elevation of Unit 22 that corresponds with the adjacent small communal bin storage areas for 4 dwellings.

Development Standards for Subdivision

8.6.1 Lot design

Objective:

That each lot:

(a) has an area and dimensions appropriate for use and development in the zone;

(b) is provided with appropriate access to a road;

(c) contains areas which are suitable for development appropriate to the zone purpose, located to avoid natural hazards; and

(d) is orientated to provide solar access for future dwellings.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Each lot, or a lot proposed in a plan of subdivision, must:</p> <p>(a) have an area of not less than 450m² and:</p> <p>(i) be able to contain a minimum area of 10m x 15m with a gradient not steeper than 1 in 5, clear of:</p> <p>a. all setbacks required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2; and</p> <p>b. easements or other title restrictions that limit or restrict development; and</p> <p>(ii) existing buildings are consistent with the setback required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2;</p> <p>(b) be required for public use by the Crown, a council or a State authority;</p> <p>(c) be required for the provision of Utilities; or</p> <p>(d) be for the consolidation of a lot with another lot provided each lot is within the same zone.</p>	<p>P1</p> <p>Each lot, or a lot proposed in a plan of subdivision, must have sufficient useable area and dimensions suitable for its intended use, having regard to:</p> <p>(a) the relevant requirements for development of buildings on the lots;</p> <p>(b) the intended location of buildings on the lots;</p> <p>(c) the topography of the site;</p> <p>(d) the presence of any natural hazards;</p> <p>(e) adequate provision of private open space; and</p> <p>(f) the pattern of development existing on established properties in the area.</p>
<p>Assessment:</p> <p>The proposed amalgamated lot complies with A1.</p>	
<p>A2</p>	<p>P2</p>

<p><i>Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a frontage not less than 12m.</i></p>	<p><i>Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must be provided with a frontage or legal connection to a road by a right of carriageway, that is sufficient for the intended use, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the width of frontage proposed, if any;</i> <i>(b) the number of other lots which have the land subject to the right of carriageway as their sole or principal means of access;</i> <i>(c) the topography of the site;</i> <i>(d) the functionality and useability of the frontage;</i> <i>(e) the ability to manoeuvre vehicles on the site; and</i> <i>(f) the pattern of development existing on established properties in the area,</i> <p><i>and is not less than 3.6m wide.</i></p>
<p>Assessment:</p> <p>The proposed consolidated lot will have a frontage in excess of 12m and complies with A2.</p>	
<p>A3</p> <p><i>Each lot, or a lot proposed in a plan of subdivision, must be provided with a vehicular access from the boundary of the lot to a road in accordance with the requirements of the road authority.</i></p>	<p>P3</p> <p><i>Each lot, or a lot proposed in a plan of subdivision, must be provided with reasonable vehicular access to a boundary of a lot or building area on the lot, if any, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the topography of the site;</i> <i>(b) the distance between the lot or building area and the carriageway;</i> <i>(c) the nature of the road and the traffic;</i> <i>(d) the anticipated nature of vehicles likely to access the site; and</i> <i>(e) the ability for emergency services to access the site.</i>
<p>Assessment:</p> <p>Complies</p>	
<p>A4</p>	<p>P4</p>

<p><i>Any lot in a subdivision with a new road, must have the long axis of the lot between 30 degrees west of true north and 30 degrees east of true north.</i></p>	<p><i>Subdivision must provide for solar orientation of lots adequate to provide solar access for future dwellings, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the size, shape and orientation of the lots;</i> <i>(b) the topography of the site;</i> <i>(c) the extent of overshadowing from adjoining properties;</i> <i>(d) any development on the site;</i> <i>(e) the location of roads and access to lots; and</i> <i>(f) the existing pattern of subdivision in the area.</i>
<p>Assessment:</p> <p>The proposal does not involve a new road. This standard does not apply.</p>	

4. Planning Scheme Codes

The site is within a mapped Bushfire-prone area but no other mapped overlays.

The proposal is considered in relation to the Bushfire Code and other relevant codes below.

Parking and Sustainable Transport Code

This Code applies to all use and development. The proposal is assessed against this code in the accompanying TIA and summarised below.

2.5 Use Standards

C2.5.1 Car parking numbers

Objective:

That an appropriate level of car parking spaces are provided to meet the needs of the use.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:</i></p> <ul style="list-style-type: none"> <i>(a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;</i> 	<p>P1</p> <p><i>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the availability of off-street public car parking spaces within reasonable walking distance of the site;</i>

<p><i>(b) the site is contained within a parking precinct plan and subject to Clause C2.7;</i></p> <p><i>(c) the site is subject to Clause C2.5.5; or</i></p> <p><i>(d) it relates to an intensification of an existing use or development or a change of use where:</i></p> <p><i>(i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or</i></p> <p><i>(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:</i></p> <p><i>$N = A + (C - B)$</i></p> <p><i>N = Number of on-site car parking spaces required</i></p> <p><i>A = Number of existing on site car parking spaces</i></p> <p><i>B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1</i></p> <p><i>C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.</i></p>	<p><i>(b) the ability of multiple users to share spaces because of:</i></p> <p><i>(i) variations in car parking demand over time; or</i></p> <p><i>(ii) efficiencies gained by consolidation of car parking spaces;</i></p> <p><i>(c) the availability and frequency of public transport within reasonable walking distance of the site;</i></p> <p><i>(d) the availability and frequency of other transport alternatives;</i></p> <p><i>(e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping;</i></p> <p><i>(f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity;</i></p> <p><i>(g) the effect on streetscape; and</i></p> <p><i>(h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.</i></p> <p>P1.2</p> <p><i>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</i></p> <p><i>(a) the nature and intensity of the use and car parking required;</i></p> <p><i>(b) the size of the dwelling and the number of bedrooms; and</i></p> <p><i>(c) the pattern of parking in the surrounding area.</i></p>
<p>Assessment:</p> <p>The development site is providing a total of 129 on-site car parking spaces, meeting the planning scheme minimum requirement, minimising the risk of overflow parking. The number of on-site car parking spaces complies with the acceptable solution of the planning scheme.</p>	

2.6 Development Standards

C2.6.1 Construction of parking areas

Objective:

That parking areas are constructed to an appropriate standard.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>All parking, access ways, manoeuvring and circulation spaces must:</i></p> <p><i>(a) be constructed with a durable all weather pavement;</i></p> <p><i>(b) be drained to the public stormwater system, or contain stormwater on the site; and</i></p> <p><i>(c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.</i></p>	<p>P1</p> <p><i>All parking, access ways, manoeuvring and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to:</i></p> <p><i>(a) the nature of the use;</i></p> <p><i>(b) the topography of the land;</i></p> <p><i>(c) the drainage system available;</i></p> <p><i>(d) the likelihood of transporting sediment or debris from the site onto a road or public place;</i></p> <p><i>(e) the likelihood of generating dust; and</i></p> <p><i>(f) the nature of the proposed surfacing.</i></p>
<p>Assessment:</p> <p>The parking areas and internal driveways will be a concrete surface, with the driveways operating with a one-way camber to direct surface water to kerbing, which will be directed to an approved stormwater drainage system. The design complies with the acceptable solution A1.</p>	

C2.6.2 Design and layout of parking areas

Objective:

That parking areas are designed and laid out to provide convenient, safe and efficient parking.

Acceptable Solutions	Performance Criteria
<p>A1.1</p> <p><i>Parking, access ways, manoeuvring and circulation spaces must either:</i></p> <p><i>(a) comply with the following:</i></p>	<p>P1</p> <p><i>All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:</i></p>

<p>(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;</p> <p>(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;</p> <p>(iii) have an access width not less than the requirements in Table C2.2;</p> <p>(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;</p> <p>(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;</p> <p>(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and</p> <p>(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or</p> <p>(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6.</p> <p>A1.2</p> <p><i>Parking spaces provided for use by persons with a disability must satisfy the following:</i></p> <p>(a) be located as close as practicable to the main entry point to the building;</p> <p>(b) be incorporated into the overall car park design; and</p> <p>(c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities. [S35]</p>	<p>(a) the characteristics of the site;</p> <p>(b) the proposed slope, dimensions and layout;</p> <p>(c) useability in all weather conditions;</p> <p>(d) vehicle and pedestrian traffic safety;</p> <p>(e) the nature and use of the development;</p> <p>(f) the expected number and type of vehicles;</p> <p>(g) the likely use of the parking areas by persons with a disability;</p> <p>(h) the nature of traffic in the surrounding area;</p> <p>(i) the proposed means of parking delineation; and</p> <p>(j) the provisions of Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking and AS 2890.2 -2002 Parking facilities, Part 2: Off-street commercial vehicle facilities.</p>
<p>Footnotes:</p> <p>[S35] Requirements for the number of accessible car parking spaces are specified in part D3 of the National Construction Code 2016.</p>	
<p>Assessment:</p> <p>The internal layout and parking areas have been designed to comply with the Australian Standard 2890.1:2004 for a residential property and parking space dimensions in the planning scheme, to ensure vehicles can easily manoeuvre within the development and enter and leave in a forward-driving direction.</p>	

There is sufficient manoeuvring width adjacent to the parking spaces, to enable all vehicles to enter and leave efficiently. All parking spaces will be located on gradient less than five percent. The internal driveways will be wide enough to accommodate two-way traffic flow, except for the small spur driveway servicing units 12 and 13 that services four parking spaces, which will be a minimum of three metres wide, complying with the width specified in the planning scheme table C2.2 for the number of parking spaces served. The enclosed garages will comply with section 5.4 of the Australian Standard 2890.1:2004. The open parking spaces will be supported with wheel stops and delineated with road marking where appropriate. Overall, the design complies with the acceptable solution A1.1(a) and (b).

C2.6.3 Number of accesses for vehicles

Objective:

That:

(a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;

(b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and

(c) the number of accesses minimise impacts on the streetscape.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p><i>The number of accesses provided for each frontage must:</i></p> <p><i>(a) be no more than 1; or</i></p> <p><i>(b) no more than the existing number of accesses,</i></p> <p><i>whichever is the greater.</i></p>	<p>P1</p> <p><i>The number of accesses for each frontage must be minimised, having regard to:</i></p> <p><i>(a) any loss of on-street parking; and</i></p> <p><i>(b) pedestrian safety and amenity;</i></p> <p><i>(c) traffic safety;</i></p> <p><i>(d) residential amenity on adjoining land; and</i></p> <p><i>(e) the impact on the streetscape.</i></p>

Assessment:

The development site will operate with the existing access onto Jetty Road, and this complies with the acceptable solution A1 (a) and (b).

C2.6.5 Pedestrian access

Objective:

That pedestrian access within parking areas is provided in a safe and convenient manner.

Acceptable Solutions	Performance Criteria
<p>A1.1</p> <p><i>Uses that require 10 or more car parking spaces must:</i></p> <p><i>(a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:</i></p> <p><i>(i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or</i></p> <p><i>(ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and</i></p> <p><i>(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.</i></p> <p>A1.2</p> <p><i>In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.</i></p>	<p>P1</p> <p><i>Safe and convenient pedestrian access must be provided within parking areas, having regard to:</i></p> <p><i>(a) the characteristics of the site;</i></p> <p><i>(b) the nature of the use;</i></p> <p><i>(c) the number of parking spaces;</i></p> <p><i>(d) the frequency of vehicle movements;</i></p> <p><i>(e) the needs of persons with a disability;</i></p> <p><i>(f) the location and number of footpath crossings;</i></p> <p><i>(g) vehicle and pedestrian traffic safety;</i></p> <p><i>(h) the location of any access ways or parking aisles; and</i></p> <p><i>(i) any protective devices proposed for pedestrian safety.</i></p>
<p>Assessment:</p> <p>Dedicated pedestrian pathways will be provided to connect all units with the existing footpath along Jetty Road. The pathways will be a concrete surface, minimum of one metre wide and separated from the driveways by kerbing where possible. Where the pathway crosses the internal driveway, painted markings will be used to delineate pedestrian priority. A 10 km/h shared speed limit will be installed at the beginning of the site, to moderate the operating speed of vehicles. The design complies with the acceptable solution.</p>	

C2.6.8 Siting of parking and turning areas

Objective:

That the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.

Acceptable Solutions	Performance Criteria
A1	P1

<p><i>Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas must be located behind the building line of buildings, excluding if a parking area is already provided in front of the building line.</i></p>	<p><i>Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas, may be located in front of the building line where this is the only practical solution and does not cause an unreasonable loss of amenity to adjoining properties, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) topographical or other site constraints;</i> <i>(b) availability of space behind the building line;</i> <i>(c) availability of space for vehicle access to the side or rear of the property;</i> <i>(d) the gradient between the front and the rear of existing or proposed buildings;</i> <i>(e) the length of access or shared access required to service the car parking;</i> <i>(f) the location of the access driveway at least 2.5m from a window of a habitable room of a dwelling;</i> <i>(g) the visual impact of the vehicle parking and access on the site;</i> <i>(h) the streetscape character and amenity;</i> <i>(i) the nature of the zone in which the site is located and its preferred uses; and</i> <i>(j) opportunities for passive surveillance of the road.</i>
<p>Assessment:</p> <p>The proposal located in the General Residential Zone does not conflict with this Standard.</p>	

Road and Railway Assets Code

This code applies to the proposed use and development that is adjacent to the East Derwent Highway and involves upgraded vehicle access. The proposal is assessed against this code in the accompanying TIA and summarised below.

Traffic generation at a vehicle crossing, level crossing or new junction (C3.5.1)

Objective:

To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction

Use Standard	Assessment
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<p>A1.4</p> <p><i>Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</i></p> <p><i>(a) the amounts in Table C3.1; or</i></p> <p><i>(b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road.</i></p>	<p>The proposed increase from 2 to 53 dwellings will involve an increase of more than the 40 vehicle movements per day under Table C3.1 and is to be assessed under P1.</p>
<p>P1</p> <p><i>Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</i></p> <p><i>(a) any increase in traffic caused by the use;</i></p> <p><i>(b) the nature of the traffic generated by the use;</i></p> <p><i>(c) the nature of the road;</i></p> <p><i>(d) the speed limit and traffic flow of the road;</i></p> <p><i>(e) any alternative access to a road;</i></p> <p><i>(f) the need for the use;</i></p> <p><i>(g) any traffic impact assessment; and</i></p> <p><i>(h) any advice received from the rail or road authority</i></p>	<p>The TIA confirms that the proposal satisfies P1 as follows:</p> <ul style="list-style-type: none"> a) The 53 residential units are estimated to generate 324 daily vehicular trips, with 33 of these trips likely to occur during the morning and evening peak periods. The two existing residential units are already generating on average 15 daily vehicle trips, with two of these occurring during the morning and evening peak periods. b) The residential units are expected to generate light vehicles less than 5.5 metres in length. These types of vehicles are associated with urban residential living, have good manoeuvrability, and are compatible with the existing vehicles using the surrounding road network. c) Jetty Road is a local residential road, built to an urban standard, has sufficient width to accommodate two-way traffic movements, and can support on-street parking. The surrounding road network is of suitable standard to accommodate the minor increase in traffic flow. There is sufficient sight distance at the existing vehicular access to enable vehicles to enter and leave the development site in a safe and efficient manner. d) Jetty Road has a posted speed limit of 50 km/h. Recent manual traffic surveys found the road is lightly trafficked, with 102 two-way traffic flow in the morning peak and 140 in the evening peak. Traffic analysis of the surrounding road network, including traffic modelling conducted at the surrounding


	<p>junctions and roundabout, indicates there is sufficient spare traffic capacity to absorb the increase in traffic, without causing adverse traffic impact, or reduction in traffic flow, or residential amenity.</p> <p>e) None</p> <p>f) Urban infill in established towns is an excellent method to increase the supply of housing, while optimising the current infrastructure and community facilities.</p> <p>g) An independent traffic assessment found no reason for this development not to proceed.</p> <p>h) No known advice.</p>
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Development Standards for Buildings or Works (C3.6)

Habitable buildings for sensitive uses within a road or railway attenuation area (C3.6.1)

Objective:

To minimise the effects of noise, vibration, light and air emissions on sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.

Development Standard	Assessment
<p>A1</p> <p><i>Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:</i></p> <p><i>(a) within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building;</i></p> <p><i>(b) an extension which extends no closer to the existing or future major road or rail network than:</i></p> <p><i>(i) the existing habitable building; or</i></p> <p><i>(ii) an adjoining habitable building for a sensitive use; or</i></p> <p><i>(c) located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the Noise Measurement Procedures Manual, 2nd edition, July 2008.</i></p>	<p>The East Derwent Highway, adjacent to the east of the site is a Category 3 road under the State Road Hierarchy with a speed limit of 80km/h.</p> <p>A 50m road attenuation area from the boundary with the highway therefore applies.</p>  <p>Road attenuation area (Source: Figure 1.1 of the Traffic Noise Assessment, NVC, 8 April 2024).</p> <p>The accompanying Traffic Noise Assessment has been prepared and demonstrates that measurements</p>

	<p>conducted at the worst-affected boundary to site resulted in an L10₁₈-hour of nominally 62.1 dBA_{adj}. This is below the criterion outlined in Table C.2 for roads, and the Acceptable Solution A1 under Clause C3.6.1 is therefore satisfied.</p>
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5. Conclusion

The proposal provides for a range of dwelling types and sizes on serviced residential land. It demonstrates the efficient use of this land consistent with the Purpose and density provisions of the General Residential Zone.

The proposal involves a small number of exceedances of the permitted building envelope for the zone. However, the proposal is considered to satisfy the relevant performance criteria under Clause 8.4.2 P3 in that the additional height is either located in the centre of the site, away from dwellings on adjoining properties (Units 24 and 25), or in the case of Unit 1, will not tangibly increase impact beyond a permitted development.

The proposal complies with the privacy and private open space provisions of the zone.

The proposal is accompanied by comprehensive Traffic and Noise assessments that confirm that the relevant provision of the Parking and Sustainable Transport and Road and Railway Assets Codes are satisfied.

The proposal is recommended for approval pursuant to Section 57 of the Act following public advertising.

I would be pleased to discuss or confirm as necessary.



Frazer Read

Principal

20 May 2024

Jo Blackwell
Senior Planner
Brighton Council
1 Tivoli Road
OLD BEACH 7017

Dear JO,

Development Application DA 2024/00061 - 24B and 38 Jetty Road Old Beach

I refer to your request for further information, 14 May 2024 and respond to Item 3 – Building Envelope.

I understand that SJM Property Developments will coordinate a response to the other items in your request.

Item 3

Please see attached amended plans that now site Unit 1 entirely within the permitted building envelope.

I provide an updated response against Clause 8.4.2 A3/P3 as follows:

8.4.2 Setbacks and building envelope for all dwellings

Objective:

The siting and scale of dwellings:

(a) provides reasonably consistent separation between dwellings and their frontage within a street;

(b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings;

(c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and

(d) provides reasonable access to sunlight for existing solar energy installations.

Acceptable Solutions	Performance Criteria
<p>A3</p> <p><i>A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:</i></p> <p><i>(a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by:</i></p> <p><i>(i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and</i></p> <p><i>(ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and</i></p> <p><i>(b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling:</i></p> <p><i>(i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or</i></p> <p><i>(ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser).</i></p>	<p>P3</p> <p><i>The siting and scale of a dwelling must:</i></p> <p><i>(a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:</i></p> <p><i>(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;</i></p> <p><i>(ii) overshadowing the private open space of a dwelling on an adjoining property;</i></p> <p><i>(iii) overshadowing of an adjoining vacant property;</i></p> <p><i>or</i></p> <p><i>(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;</i></p> <p><i>(b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and</i></p> <p><i>(c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:</i></p> <p><i>(i) an adjoining property; or</i></p> <p><i>(ii) another dwelling on the same site.</i></p>
<p>Assessment:</p> <p>The accompanying amended plans and 3D drapes of the building envelope confirm that the proposal involves some minor exceedances of the permitted building envelope under A3 and must therefore be assessed under P3.</p> <p>Specifically, the ridge line of Units 23 and 24, located in the centre of the site, exceed the 8.5m permitted maximum under A3a)ii).</p>	

The 3D drape also shows that the eaves of Units 21, 40, 41, 42, 43, 45 and 46 each include a minor horizontal protrusion of less than 900mm and comply with A3.

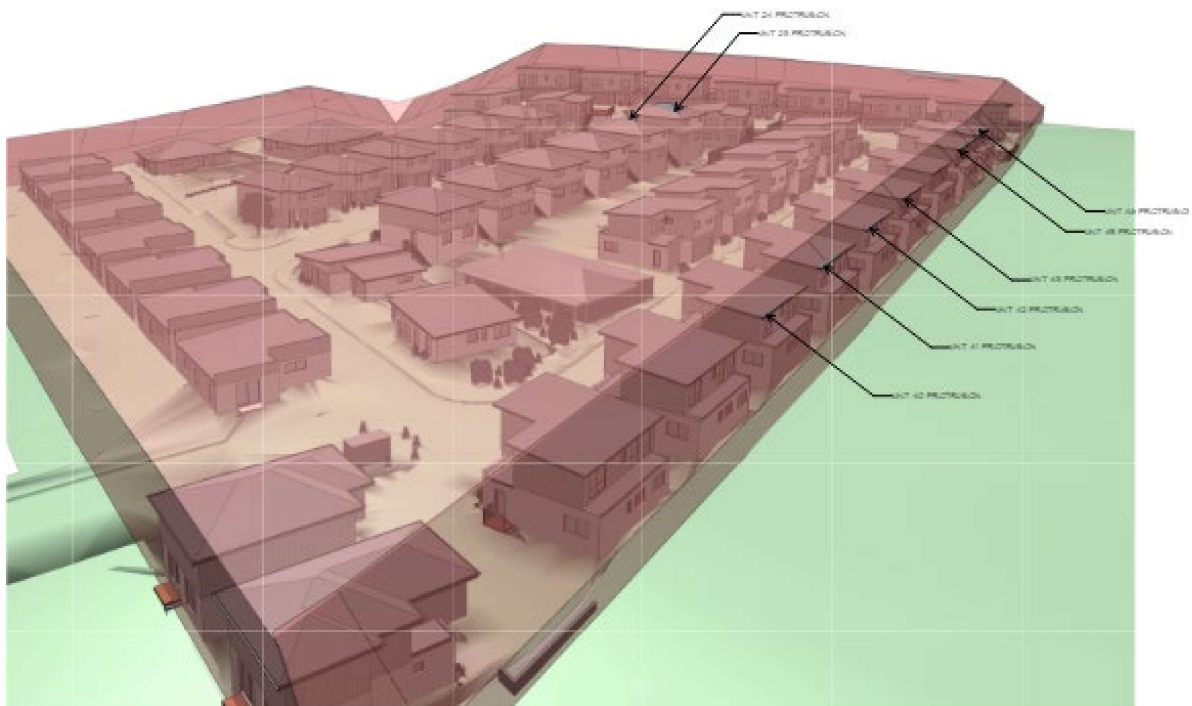
Unit 1 has now been amended to fit within the permitted envelope.

P3 assessment:

Having regard to subsequent full Court decision, *Boland v Clarence City Council 2021, TASFC 5*, it is understood that some regard may be had to the acceptable solution.

In this case, it is only the ridgelines of Units 23 and 24 that do not comply with the permitted building envelope under A3. All other impacts of the proposal are equal to or less than what can occur from the permitted building envelope under the acceptable solution A3. Those impacts are therefore considered acceptable.

In relation to Units 23 and 24, because they are sited in the middle of the site, the additional height will have no shadowing, privacy or visual impact to a dwelling on and adjoining property. No existing solar energy installation either on the site or adjoining will be impacted by the proposal. Therefore, to the extent that the proposal does not satisfy A3 in relation to these dwellings (Units 23 and 24), the proposal is considered to satisfy P3.



I would be pleased to discuss as necessary.

Yours sincerely,

A handwritten signature in blue ink, appearing to read 'Frazer Read'. The signature is fluid and cursive, with the first name 'Frazer' written in a larger, more prominent script than the last name 'Read'.

Frazer Read

Principal

All Urban Planning Pty Ltd